



CORPORATION OF THE CITY OF VERNON

ADVISORY PLANNING COMMITTEE

WEDNESDAY, FEBRUARY 14, 2018

4:00 pm – OKANAGAN LAKE ROOM

A G E N D A

1. **ADOPTION OF AGENDA**
2. **ADOPTION OF MINUTES**
January 23, 2018 (attached)
3. **NEW BUSINESS**
 - a) **ZON00295** – Rezoning application for 4005 Pleasant Valley Road
 - b) **DVP00418** – Development Variance Permit application for 1803 31A Street
4. **INFORMATION ITEMS**
 - a) The Staff Liaison provided an update of APC items discussed at the February 13, 2018 Council meeting.
5. **NEXT MEETING**
The next meeting is tentatively scheduled for Tuesday, February 27, 2018.
6. **ADJOURNMENT**



THE CORPORATION OF THE CITY OF VERNON

MINUTES OF ADVISORY PLANNING COMMITTEE MEETING

HELD

TUESDAY, JANUARY 23, 2018

PRESENT: VOTING

Mark Longworth, Chair
Jamie Paterson
Barry Beardsell
Don Schuster
Monique Hubbs-Michiel
Ian Murphy
Vicki Topping

NON VOTING

Councillor Cunningham
Councillor Spiers
Corbin Kelley, Youth Member

ABSENT: Joanne Georgeson
Doug Neden, Vice-Chair
Natalie Lussin
Lisa Kongsdorf

STAFF: Craig Broderick, Manager Current Planning
Ed Stranks, Manager, Engineering Development Services
Kim Flick, Director, Community Infrastructure and Development Services
Roy Nuriel, Planner, Economic Development
Cleo Corbett, Long Range Planner
Janice Nicol, Legislative Committee Clerk

ORDER

The Chair called the meeting to order at 4:00 p.m.

**ADOPTION OF
AGENDA**

Moved by Jamie Paterson, seconded by Monique Hubbs-Michiel :

THAT the agenda of the Advisory Planning Committee meeting for Tuesday, January 23, 2018 be adopted.

CARRIED.

ADOPTION OF MINUTES

Moved by Don Schuster, seconded by Monique Hubbs-Michiel;

THAT the minutes for the Advisory Planning Committee meeting of Tuesday, January 9, 2018 be adopted.

CARRIED.

Barry Beardsell entered the meeting at 4:01 p.m.

Natalie Lussin entered the meeting at 4:06 p.m.

NEW BUSINESS:

DEVELOPMENT VARIANCE PERMIT #00419 – 3908 32 STREET

The Advisory Planning Committee reviewed Development Variance Permit application DVP00419 located at 3908 32 Street.

The following concerns were noted:

- The rationale for the reduced parking.
- The closeness of the proposed patio to the highway.
- Any future building shouldn't be permitted in the patio area.

Moved by Don Schuster, seconded by Vicki Topping;

THAT the Advisory Planning Committee recommends that Council support the development variance permit application to vary the following sections of Zoning Bylaw #5000 to allow for the construction of a seasonal patio restaurant on Lot A, Plan 32682, Sec 3, Twp 8, ODYD (3908 – 32nd Street):

- a) To vary the minimum front yard setback from 6.0m to 1.4m (Sec.10.10.5); and
- b) To vary the minimum required parking spaces from 52 spaces to 43 spaces (Sec.7, Table 7.1).

AND FURTHER, that the Advisory Planning Committee recommends that Council's support of DVP00419 is subject to the following:

- a) That the site, floor and elevation plans shown as Attachments 2, 3 and 5 in the report titled "Development Variance Permit Application for 3908 – 32nd Street" dated January 15, 2018 by the Economic Development Planner are to be attached to and form part of Development Variance Permit #DVP00419 as Schedule 'A'.

CARRIED.**DEVELOPMENT
VARIANCE PERMIT
#00420 – 5400 WILLOW
DRIVE**

The Advisory Planning Committee reviewed Development Variance Permit application DVP00420 located at 5400 Willow Drive. The Municipal Tech - Development provided an overview of the presentation. The following points were noted:

- Suggestion to have increased on-street parking although Willow Drive is extremely narrow, would be difficult to provide on-street parking requirements.
- In this residential area, there is no required dedicated on-street parking.

Moved by Jamie Paterson, seconded by Monique Hubbs-Michiel

THAT the Advisory Planning Committee recommends that Council support the development variance permit application (DVP00420) to vary Subdivision and Development Servicing Bylaw #3843 Schedule A requirements to upgrade the road works in Willow Drive adjacent to Lot A, Plan EPP31464, District Lot 75, ODYD, by not requiring asphalt widening, new curb, gutter, sidewalk or streetlights as a condition of development approval and return the funds collected for the works contribution.

CARRIED with Ian Murphy opposed.

**OFFICIAL COMMUNITY
PLAN AMENDMENT
#00072 – 5902
PLEASANT VALLEY
ROAD**

The Advisory Planning Committee reviewed Official Community Plan amendment application OCP00072 located at 5902 Pleasant Valley Road. The Long Range Planner provided an overview of the presentation. The following points were noted:

- Concerns about road access and elevation.
- Concern that drainage and other technical issues need to be dealt with regardless of low or medium density development.

Moved by Barry Beardsell, seconded by Monique Hubbs-Michiel;

THAT the Advisory Planning Committee recommends that Council NOT support the application to amend the OCP land use designation for Lot 4, Section 11, Township 8, ODYD, Plan 4165 (5902 Pleasant Valley Road) from the Residential – Regional District of North Okanagan Electoral Areas B & C Official Community Plan land use designation to Residential – Medium Density;

AND FURTHER that the Advisory Planning Committee recommends that Council support amending the OCP land use designation for Lot 4, Section 11, Township 8, ODYD, Plan 4165 (5902 Pleasant Valley Road) from the Residential – Regional District of North Okanagan Electoral Areas B & C Official Community Plan land use designation to Residential – Low Density and to include the subject property within Development District #2 – Neighbourhood District and Development Permit Area #2 subject to:

1. Dedication of a 3 metre road right of way along the southern most boundary of the property for emergency access, a walkway connection and for drainage works;
2. That prior to any construction on the site, an Archeological Impact Assessment be conducted on the lower south east half of the property as outlined in Figure 3.

CARRIED, with Don Schuster opposed.

INFORMATION ITEMS:

The Staff Liaison provided an update of APC related items discussed at the January 22, 2018 Council meetings as follows:

- DVP00412 – NOYFSS Building on 31 Street and 32 Avenue – Alternative 3 approved – (this alternative came forward after report was viewed by APC), issued once all conditions are satisfied.
- McMechan Reservoir proposal was received at Committee of the Whole Meeting – Public Open House scheduled for Jan. 31 at the Re.c Centre.

NEXT MEETING

The next regular meeting of the Advisory Planning Committee is scheduled for **Wednesday**, February 14, 2018.

ADJOURNMENT

The meeting of the Advisory Planning Committee adjourned at 4:42 p.m.

CERTIFIED CORRECT:

_____ **Chair**



THE CORPORATION OF THE CITY OF VERNON REPORT TO COUNCIL

SUBMITTED BY: Carie Liefke, Planning Assistant

COUNCIL MEETING: REG COW I/C

COUNCIL MEETING DATE: March 12, 2018

REPORT DATE: February 7, 2018

FILE: ZON00295

SUBJECT: REZONING AMENDMENT FOR 4005 PLEASANT VALLEY ROAD

PURPOSE:

To review the rezoning application which proposes to rezone the subject property located at 4005 Pleasant Valley Road in order to allow a low density multi-family development.

RECOMMENDATION:

THAT Council support the application (ZON00295) to rezone Part Lot 20 (Plan B3444) Section 2 Township 8 ODYD Plan 474 (4005 Pleasant Valley Road) from R2 – Large Lot Residential to R5 – Four-plex Housing Residential in order to allow a low density multi-family development.

ALTERNATIVES & IMPLICATIONS:

1. THAT Council support the application (ZON00295) to rezone Part Lot 20 (Plan B3444) Section 2 Township 8 ODYD Plan 474 (4005 Pleasant Valley Road) from R2 – Large Lot Residential to R5 – Four-plex Housing Residential in order to allow a low density multi-family development, subject to the following conditions: *(to be cited by Council)*.

Note: This alternative supports the rezoning subject to conditions cited by Council.

2. THAT Council not support the application (ZON00295) to rezone Part Lot 20 (Plan B3444) Section 2 Township 8 ODYD Plan 474 (4005 Pleasant Valley Road) from R2 – Large Lot Residential to R5 – Four-plex Housing Residential which would have allowed a low density multi-family development.

Note: Should Council not support the rezoning, the current zoning district would remain in place and the property could either be developed with one single family dwelling or subdivided into several lots meeting the R2 – Large Lot Residential subdivision regulations.

ANALYSIS:

A. Committee Recommendations:

At its meeting of February 14, 2018, the Advisory Planning Committee passed the following resolution:

.....

B. Rationale:

1. The subject property is located at 4005 Pleasant Valley Road (Figures 1 and 2). The land has been designated as Residential Low Density within the Official Community Plan and is within the R2 – Large Lot Residential zoning district as shown on the attached zoning map excerpt (Attachment 1).

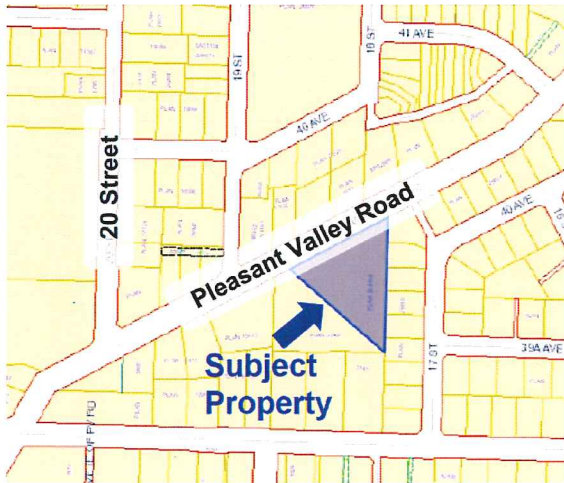


Figure 2: Property Location



Figure 1: Property Aerial View

2. The subject property is currently undeveloped.
3. The application requests that the land be rezoned from R2 – Large Lot Residential to R5 – Four-plex Housing Residential (Attachment 2) in order to develop a low density multi-family development. As the land is approximately 4000m², a maximum of 12 units could be constructed on the land.
4. The City currently owns the property and has expressed interest in having the property developed. At its Regular Meeting of January 8, 2018, Council declassified the following resolution from its In Camera meeting of September 18, 2017:

“THAT Council support, in principle, the business plan for an affordable housing development on City owned land located at 4005 Pleasant Valley Road submitted as part of the delegation from Ms. Annette Sharkey, Executive Director of the Social Planning Council of the North Okanagan, at Council’s In Camera meeting of September 6, 2017;

AND FURTHER, that Council direct Administration to prepare, in conjunction with the Social Planning Council of the North Okanagan, a zoning amendment proposal for the City owned property located at 4005 Pleasant Valley Road from R2 – Large Lot Residential to R5 – Four-plex Housing Residential, and associated agreements, for further consideration.”

5. As per the delegation form the Social Planning Council of the North Okanagan, the development proposal is for 12 semi-detached units in a series of 6 buildings as shown on the preliminary site plan and elevation drawing (Attachment 3).
6. The following table provides a basic zoning analysis of the regulations applicable to the use and development of the site.

Bylaw Regulation	Current Zone – R2	Proposed Zone – R5
Primary Uses	single detached housing	single detached housing duplex housing semi-detached housing three-plex housing four-plex housing seniors housing group home, major
Site Coverage	40% building 50% all impermeable	40% building 50% all impermeable
Floor Space Ratio	n/a	0.6

Density	one unit per lot; 557 sq.m. minimum lot size (approx.. 18 units per hectare)	30 units per hectare maximum
Building Height	lesser of 10.0m or 2.5 storeys	lesser of 10.0m or 2.5 storeys
Setbacks: front	7.5m	4.0m (except 6.0m from garage to back of curb/sidewalk)
side	2.5m (or 3.0m if no attached garage)	2.0m for 1.0/1.5 storey building or 2.5m for 2.0/2.5 storey building
rear	7.5m	6.0m for 1.0/1.5 storey building or 7.5m for 2.0/2.5 storey building
Private Open Space	n/a	25m ² per unit
Parking	2 stalls per unit	2 stalls per unit

7. Administration supports the requested rezoning for the following reasons:

- a. The proposed zoning district would provide for a more efficient use of the land.
- b. The proposed multi-family development would allow for infill in an area of the city with existing urban services (collector road, existing transit route, schools, water, sewer).
- c. A multi-family development on this site would have a minimal impact on neighbourhood views due to the topography of the site and that the maximum building height permitted corresponds with the surrounding land use.

C. Attachments:

Attachment 1 – Zoning Bylaw #5000 Schedule A zoning map excerpt

Attachment 2 – Zoning Bylaw #5000 excerpts: R2 – Large Lot Residential zoning district and R5 – Four-plex Housing Residential zoning district

Attachment 3 – preliminary site plan and elevation drawing

D. Council’s Strategic Plan 2015 – 2018 Goals/Deliverables:

The subject application involves the following objectives in Council’s Strategic Plan 2015 – 2018:

- Deliver efficient, effective and proactive municipal services.
- Support sustainable neighbourhoods be implementing the OCP.
- Work with community partners and the Affordable Housing Advisory Committee to create more affordable housing.

E. Relevant Policy/Bylaws/Resolutions:

1. The Official Community Plan (OCP) designates the property as Residential Low Density. The R5 – Four-plex Housing Residential zoning district requested conforms with the OCP designation. OCP policies which support this rezoning request include:

Policy # 7.5 → The following housing forms and maximum density are associated with Low Density classification: 30 units per hectare; Single family detached, semi-detached, duplex and row housing.

Policy # 7.6 → Ground oriented multiple family developments are strongly encouraged in designated areas in the City Centre and Neighbourhood Districts.

Policy # 20.8 → Encourage affordable housing units, either ownership or rental, within the context of market housing developments through the use of covenants for housing agreements.

Policy # 20.9 → Work in partnership with community agencies, non-profit organizations, senior levels of government and the business community in the provision of affordable housing, special needs housing and emergency shelter and housing.

Policy # 20.10 → Where appropriate, lease City owned land for the provision of units for those in core housing need or require emergency shelter.

BUDGET/RESOURCE IMPLICATIONS:

N/A

Prepared by:

Approved for submission to Council:

Right-click to sign with *DocuSign*

Carie Liefke
Planning Assistant

Will Pearce, CAO

Date: _____

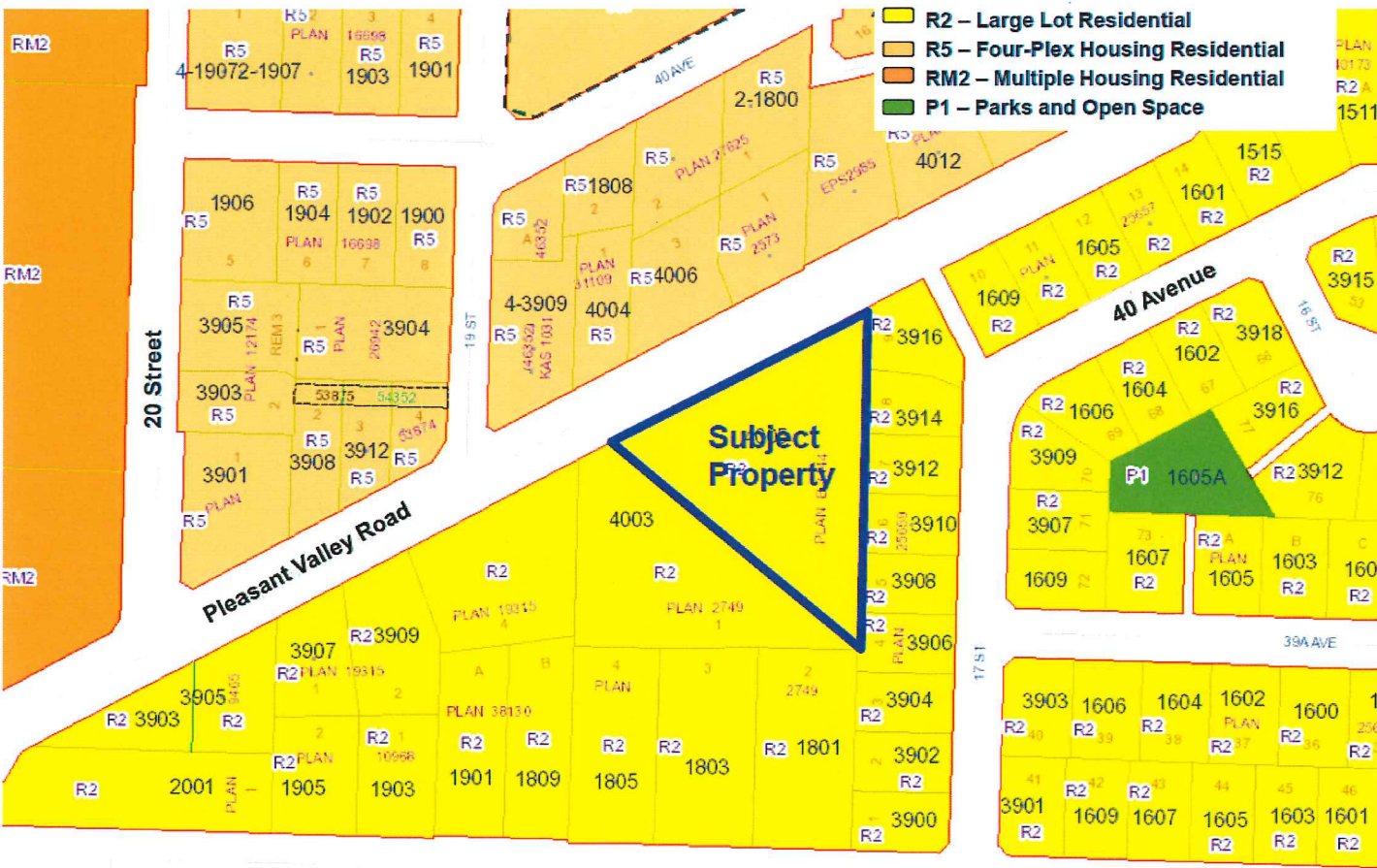
Right-click to sign with *DocuSign*

Kim Flick
Director, Community Infrastructure and Development

REVIEWED WITH

- | | | |
|--|---|---|
| <input type="checkbox"/> Corporate Services | <input type="checkbox"/> Operations | <input checked="" type="checkbox"/> Current Planning |
| <input type="checkbox"/> Bylaw Compliance | <input type="checkbox"/> Public Works/Airport | <input type="checkbox"/> Long Range Planning & Sustainability |
| <input type="checkbox"/> Real Estate | <input type="checkbox"/> Facilities | <input type="checkbox"/> Building & Licensing |
| <input type="checkbox"/> RCMP | <input type="checkbox"/> Utilities | <input type="checkbox"/> Engineering Development Services |
| <input type="checkbox"/> Fire & Rescue Services | <input type="checkbox"/> Recreation Services | <input type="checkbox"/> Infrastructure Management |
| <input type="checkbox"/> Human Resources | <input type="checkbox"/> Parks | <input type="checkbox"/> Transportation |
| <input type="checkbox"/> Financial Services | | <input type="checkbox"/> Economic Development & Tourism |
| <input checked="" type="checkbox"/> COMMITTEE: APC (Feb.14/18) | | |
| <input type="checkbox"/> OTHER: | | |

ATTACHMENT 1



Zoning Designation

9.3 R2 : Large Lot Residential

9.3.1 Purpose

The purpose is to provide a **zone** for **single detached housing**, and compatible **secondary uses**, on large sized urban serviced **lots**. The R2c sub-zoning district allows for **care centre, major** as an additional use. The R2h sub-zoning district allows for **home based business, major** as an additional use. (*Bylaw 5467*)

9.3.2 Primary Uses

- **care centre, major** (*use is only permitted with the R2c sub-zoning district*)
- **single detached housing**

9.3.3 Secondary Use

- **boarding rooms**
- **bed and breakfast homes** (in single detached housing only) (*Bylaw 5498*)
- **care centres, minor**
- **group home, minor**
- **home based businesses, minor**
- **home based businesses, major** (*use is only permitted with the R2h sub-zoning district*)
- **secondary suites**
- **seniors supportive housing**

9.3.4 Subdivision Regulations

- Minimum **lot width** is 18.0m.
- Minimum **lot area** is 557m², or 10,000m² if not serviced by a **community sewer system**.

9.3.5 Development Regulations

- Maximum **site coverage** is 40% and together with driveways, parking areas and **impermeable surfaces** shall not exceed 50%.
- Maximum **height** is the lesser of 10.0m or 2.5 **storeys**, except it is 4.5m for **secondary buildings** and **secondary structures**.
- Minimum **front yard** is 5.0m.
- Minimum **side yard** is 1.5m, except it is 5.0m from a **flanking street**. Where there is no direct vehicular access to the **rear yard** or to an attached garage or **carport**, one **side yard** shall be at least 3.0m.
- Minimum **rear yard** is 7.5m, except it is 1.0m for **secondary buildings**. Where the **lot width** exceeds the **lot depth**, the minimum **rear yard** is 4.5m provided that one **side yard** shall have a minimum width of 4.5m.
- The maximum **height** of any vertical wall element facing a **front, flanking** or **rear yard** (including **walkout basements**) is the lesser of 6.5m or 2.5 **storeys**, above which the **building** must be **set back** at least 1.2m.

9.3.6 Other Regulations

- There shall be no more than one **single detached house** per **lot**.
- Where **development** has access to a rear **lane**, vehicular access to the **development** is only permitted from the rear **lane**.
- For **seniors supportive housing**, a safe drop-off area for patrons shall be provided on the **site**.

- **Seniors supportive housing** shall be for no more than four residents. *(Bylaw 5467)*
- In addition to the regulations listed above, other regulations may apply. These include the general **development** regulations of Section 4 (secondary **development, yards**, projections into **yards**, lighting, agricultural setbacks, etc.); the specific use regulations of Section 5; the **landscaping** and fencing provisions of Section 6; and, the parking and loading regulations of Section 7.
- As per Section 4.10.2 - All **buildings and structures, excluding perimeter fencing (garden walls and fences)** on **lots abutting** City Roads as identified on Schedule "B" shall not be sited closer to the City Road than the setback as per the appropriate zone measured from the offset Rights of Way as illustrated on Schedule "B".
(Bylaw 5440)

9.6 R5 : Four-plex Housing Residential

9.6.1 Purpose

The purpose is to provide a **zone** for the **development** of a maximum of four ground oriented **dwelling** units in the form of **single detached, semi-detached, duplex, three-plex** or **four-plex housing** on urban services. The R5c sub-zoning district allows for **care centre, major** as an additional use. The R5h sub-zoning district allows for **home based business, major** as an additional use. *(Bylaw 5467)*

9.6.2 Primary Uses⁴

- **care centre, major** *(use is only permitted with the R5c sub-zoning district)*
- **duplex housing**
- **four-plex housing**
- **group home, major**
- **semi-detached housing**
- **single detached housing**
- **three-plex housing**
- **seniors housing**

9.6.3 Secondary Uses

- **boarding rooms**
- **care centres, minor**
- **home based businesses, minor**
- **home based businesses, major** *(in single detached housing only) (use is only permitted with the R5h sub-zoning district)*
- **secondary suites (in single detached housing only)**
- **seniors assisted housing**
- **seniors supportive housing**

9.6.4 Subdivision Regulations

- Minimum **lot width** is 20.0m, except it is 22.0m for a **corner lot**.
- Minimum **lot depth** is 30.0m.
- Minimum **lot width** for single detached housing is 14.0m, except it is 16.0m for a **corner lot**.
- Minimum **lot area** for single detached housing is 450m².
- Minimum **lot area** is 700m², except it is 800m² for a **corner lot**, or 10,000m² if not serviced by a **community sewer system**. *(Bylaw 5339)*

9.6.5 Party Wall Subdivision Regulations

Lot Type	Minimum Lot area		Minimum Lot Width	
	interior	corner	interior	corner
Semi-Detached Housing	350m ²	400m ²	10.0m	12.0m
Three-Plex Housing	235m ²	285m ²	7.0m	9.0m
Four-Plex Housing	175m ²	225m ²	7.0m	9.0m

9.6.6 Development Regulations

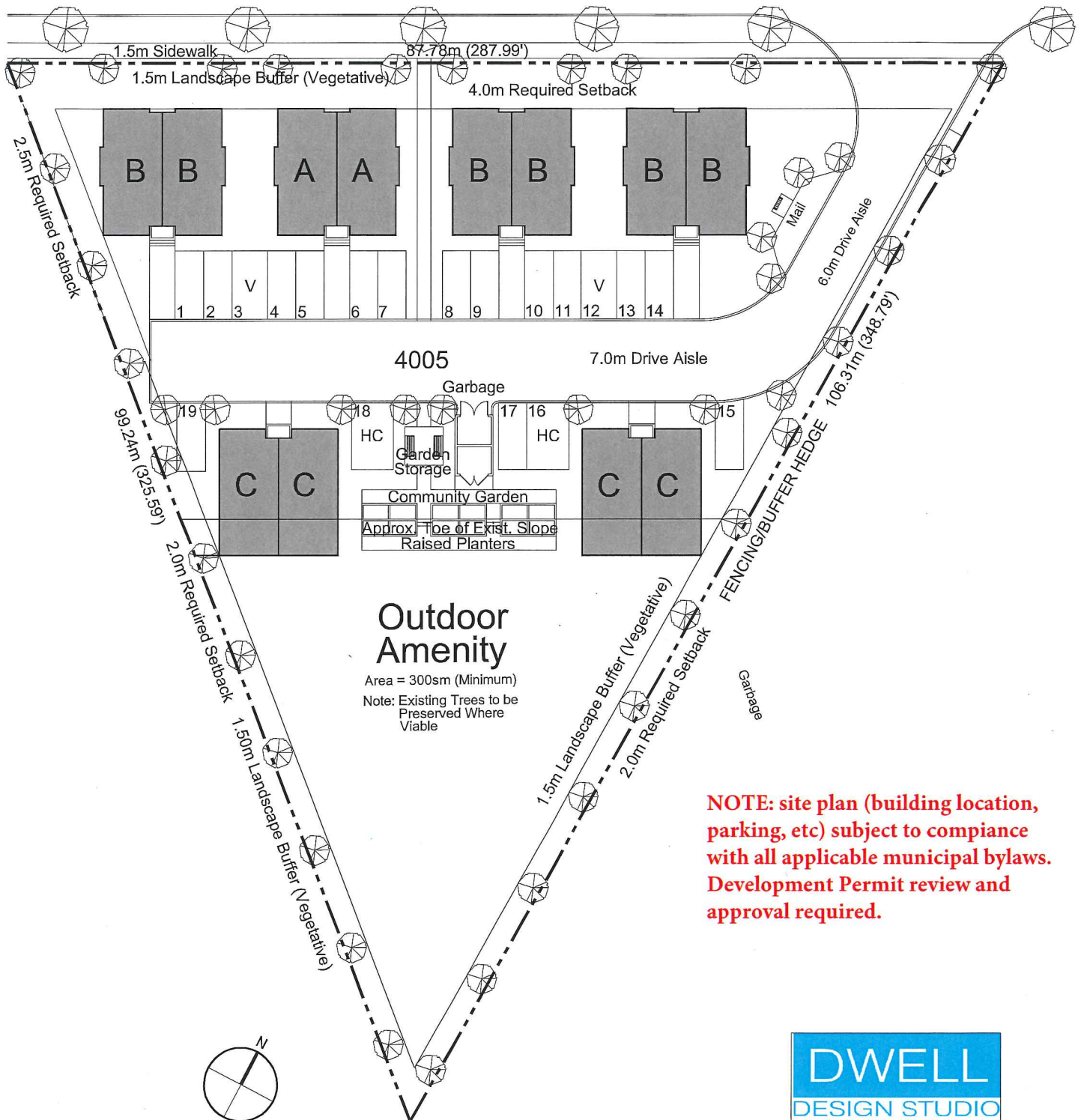
- Maximum **site coverage** is 40% and together with driveways, parking areas and **impermeable surfaces** shall not exceed 50%.

- Maximum **floor space ratio** is 0.6.
- Maximum **height** is the lesser of 10.0m or 2.5 **storeys**, except it is 4.5m for **secondary buildings** and **secondary structures**.
- Minimum **front yard** is 4.0m, except it is 6.0m for a garage or **carport** to the back of curb or sidewalk for a front entry garage, or it is 0.6m to the side of the garage and 2.6m to the front building façade for side-entry garage and driveway layouts.
- Minimum **side yard** is 2.0m for a 1 or 1.5 **storey** portion of a **building** or a **secondary building or structure** and 2.5m for a 2 or 2.5 **storey** portion of a **building**, except it is 4.0m from a **flanking street** unless there is a garage accessed from the **flanking street**, it is 4.0m or it is 2.6m to the building for a side-entry garage and driveway from a flanking street and at least 6.0m from the back of curb or sidewalk. Where there is no direct vehicular access to the **rear yard** or to an attached garage or **carport**, one **side yard** shall be at least 3.0m. The minimum **side yard** setback for shared interior **party walls** shall be 0.0m. The minimum **side yard** setback for **single detached housing** is 1.5m, except it is 4.0m from a **flanking street** unless there is a garage accessed from the **flanking street**, it is 4.0m or it is 2.6m to the building for a side-entry garage and driveway from a **flanking street** and at least 6.0m from the back of curb or sidewalk.
- Minimum **rear yard** is 6.0m for a 1 or 1.5 **storey** portion of a **building** and 7.5m for a 2 or 2.5 **storey** portion of a **building**, except it is 1.0m for **secondary buildings**.
- The maximum **height** of any vertical wall element facing a **front, flanking or rear yard** (including **walkout basements**) is the lesser of 6.5m or 2.5 **storeys**, above which the **building** must be **set back** at least 1.2m.
- Maximum **density** is 30 units per gross hectare (12 units/gross acre).
- Maximum four **dwelling** units located in a **building**, with each unit having a minimum width of 6.5m. (*Bylaw 5339*)

9.6.7 Other Regulations

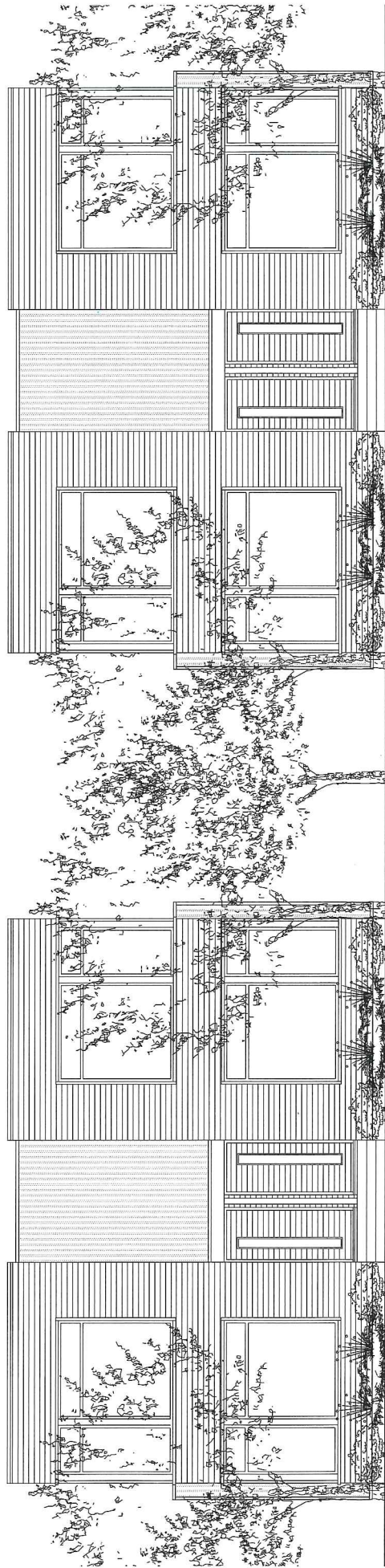
- In order for bareland strata **developments** to be consistent with the character of the surrounding neighborhood, the strata plan shall be considered as one **site** for defining the overall use, **density** and **site coverage**.
- The above noted **subdivision** and **development** regulations shall be applied to each strata **lot** within the strata plan.
- A minimum area of 25m² of **private open space** shall be provided per **dwelling**.
- Where **development** has access to a rear **lane**, vehicular access to the **development** is only permitted from the rear **lane**.
- For **seniors assisted housing, seniors housing and seniors supportive housing**, a safe drop-off area for patrons shall be provided on the **site**.
- For strata developments, common recreation buildings, facilities and amenities may be included in the strata plan. Recreational buildings shall be treated as **secondary buildings** for the purpose of determining the **height** and **setbacks** of the **building** as specified in each **zone**.
- For multi-unit residential housing, one **office** may be operated for the sole purpose of the management and operation of the multi-unit residential **development**.
- In addition to the regulations listed above, other regulations may apply. These include the general **development** regulations of Section 4 (**secondary development, yards**, projections into **yards**, lighting, agricultural setbacks, etc.); the specific use regulations of Section 5; the **landscaping** and fencing provisions of Section 6; and, the parking and loading regulations of Section 7.
- As per Section 4.10.2 - All **buildings** and **structures, excluding perimeter fencing (garden walls and fences)** on **lots abutting** City Roads as identified on Schedule "B" shall not be sited closer to the City Road than the setback as per the appropriate zone measured from the offset Rights of Way as illustrated on Schedule "B".
(*Bylaw 5440*)

Pleasant Valley Road



NOTE: site plan (building location, parking, etc) subject to compliance with all applicable municipal bylaws. Development Permit review and approval required.





NOTE: building elevations subject to compliance with all applicable municipal bylaws. Development Permit review and approval required.



THE CORPORATION OF THE CITY OF VERNON REPORT TO COUNCIL

SUBMITTED BY: Roy Nuriel
Economic Development Planner

COUNCIL MEETING: REG COW I/C
COUNCIL MEETING DATE: February 26, 2018
REPORT DATE: February 5, 2018
FILE: DVP00418

SUBJECT: DEVELOPMENT VARIANCE PERMIT APPLICATION FOR 1803 – 31A STREET

PURPOSE:

To review the development variance permit application for 1803 – 31A Street to vary sections of Zoning Bylaw #5000 and of Subdivision and Development Servicing Bylaw #3843 in order to construct a four and a half storey, 43 unit rental apartment building.

RECOMMENDATION:

THAT Council support Development Variance Permit Application #DVP00418 to vary the following sections of Zoning Bylaw #5000 to allow for a four and a half storey, 43 unit rental apartment to be constructed on Lot 1, Plan 21336, Sec 34, Twp 9, ODYD (1803 – 31A Street):

- a) to vary the minimum yard setbacks (Section 9.12.5) in accordance with the following and as shown on Schedule 'A':
 - i. front yard: from 4.5m to 0.0m for the parkade retaining wall and building;
 - ii. side yard: from 4.5m to 0.3m and 0.0m for the parkade retaining wall;
 - iii. south side yard: from 4.5m to 1.7m for the parkade retaining wall and building;
 - iv. rear yard: from 9.0m to 0.7m and 4.4m for the parkade retaining wall;
- b) to vary the minimum front, side and rear landscape buffer (Section 6.6.2, Table 6.1) from 1.5m to 0.0m as shown on Schedule 'A';
- c) to vary the minimum number of required off-street parking spaces from 71 spaces to 54 spaces (Section 7, Table 7.1); and
- d) to vary Schedule B, to reduce the setback based on required right of way for 31A Street from 18.5m to 15.8m.

AND FURTHER, that Council support Development Variance Permit Application #DVP00418 to vary the following section of Subdivision and Development Servicing Bylaw #3843 on the property described as Lot 1, Plan 21336, Sec 34, Twp 9, ODYD (1803 – 31A Street):

- a) to vary the off-site works (Schedule A – Level of Service) standards to asphalt widening of 0.8m, new upright concrete curb and gutter and 1.5m wide concrete sidewalk in 31A Street adjacent to the property.

AND FURTHER, that Council support of DVP00418 is subject to the following:

- a) The owner is to dedicate road right-of-way widening of approximately 2.1m on Lot 1, Plan 21336, Sec 34, Twp 9, ODYD (1803 – 31A Street); and
- b) That the site, floor, elevations, landscaping plans, parking study and the Schedule 'A' noted as Attachments 1 to 8 inclusive in the report titled "Development Variance Permit Application for 1803 – 31A Street" and dated February 5, 2018 by the Economic Development Planner be attached to and form part of DVP00418 as Schedule 'A'.

ALTERNATIVES & IMPLICATIONS:

1. THAT Council support Development Variance Permit Application #DVP00418 to vary the following sections of Zoning Bylaw #5000 to allow for a four and half storey, 43 unit rental apartment to be constructed on Lot 1, Plan 21336, Sec 34, Twp 9, ODYD (1803 – 31A Street):
 - a) to vary the minimum yard setbacks (Section 9.12.5) in accordance with the following and as shown on Schedule 'A':
 - i. front yard: from 4.5m to 0.0m for the parkade retaining wall and building;
 - ii. side yard: from 4.5m to 0.3m and 0.0m for the parkade retaining wall;
 - iii. south side yard: from 4.5m to 1.7m for the parkade retaining wall and building;
 - iv. rear yard: from 9.0m to 0.7m and 4.4m for the parkade retaining wall;
 - b) to vary the minimum front, side and rear landscape buffer (Section 6.6.2, Table 6.1) from 1.5m to 0.0m as shown on Schedule 'A';
 - c) to vary the minimum number of required off-street parking spaces from 71 spaces to 54 spaces (Section 7, Table 7.1); and
 - d) to vary Schedule B, to reduce the setback based on required right of way for 31A Street from 18.5m to 15.8m.

AND FURTHER, that Council support Development Variance Permit Application #DVP00418 to vary the following section of Subdivision and Development Servicing Bylaw #3843 on the property described as Lot 1, Plan 21336, Sec 34, Twp 9, ODYD (1803 – 31A Street):

- a) to vary the off-site works (Schedule A – Level of Service) standards to asphalt widening of 0.8m, new upright concrete curb and gutter and 1.5m wide concrete sidewalk in 31A Street adjacent to the property.

AND FURTHER, that Council support of DVP00418 is subject to the following:

- a) The owner is to dedicate road right-of-way widening of approximately 2.1m on Lot 1, Plan 21336, Sec 34, Twp 9, ODYD (1803 – 31A Street);
- b) *(to be cited by Council)*; and
- c) That the site, floor, elevations, landscaping plans, parking study and the Schedule 'A' noted as Attachments 1 to 8 inclusive in the report titled "Development Variance Permit Application for 1803 – 31A Street" and dated February 5, 2018 by the Economic Development Planner be attached to and form part of DVP00418 as Schedule 'A'.

Note: This alternative supports the development variance application as requested by the applicant, subject to any other conditions cited by Council.

2. THAT Council not support Development Variance Permit Application #DVP00418 to vary Zoning Bylaw #5000 to allow for a four and half storey, 43 rental unit apartment to be constructed on Lot 1, Plan 21336, Sec 34, Twp 9, ODYD (1803 – 31A Street);

AND FURTHER, that Council not support Development Variance Permit Application #DVP00418 to vary the following section of Subdivision and Development Servicing Bylaw #3843 on the property described as Lot 1, Plan 21336, Sec 34, Twp 9, ODYD (1803 – 31A Street).

Note: This alternative does not support the development variance application. The owner would have to develop the property in accordance with the bylaws.

ANALYSIS:

A. Committee Recommendations:

At its meeting of February 14, 2018, the Advisory Planning Committee adopted the following resolution:

“ ”

B. Rationale:

1. The subject property at 1803 – 31A Street is located in the Hospital Hill neighbourhood (Figures 1 and 2). The property has been vacant since 2011 after a fire destroyed the four storey, 30 unit apartment building. The site is designated Residential – High Density in the Official Community Plan (OCP) and zoned RH1: Low – Rise Apartment Residential as per Zoning Bylaw #5000.

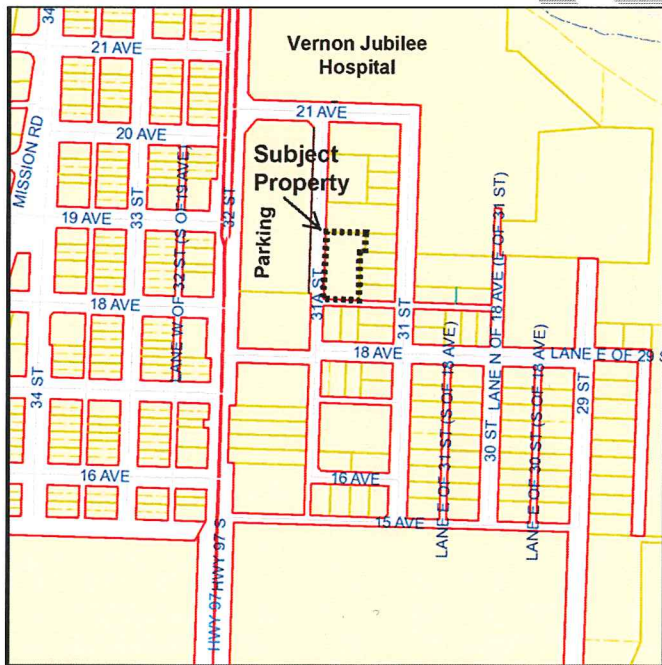


Figure 1 – Property Location Map

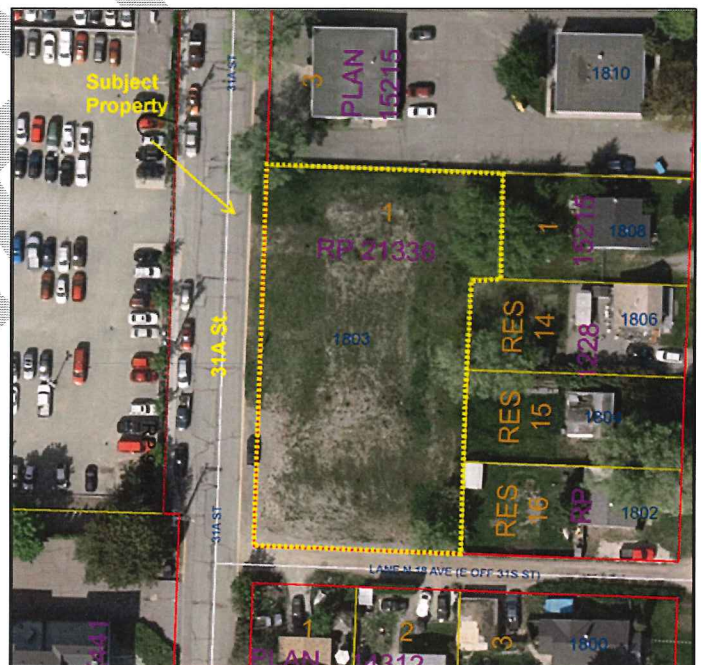


Figure 2 – Aerial Photo of Property

2. The subject application is to vary the following sections of Zoning Bylaw #5000 and Subdivision and Development Servicing Bylaw #3843 in order to construct a four and a half storey, 43 rental unit apartment building at 1803 – 31A Street (Attachments 1- 7):

Zoning Bylaw #5000

Setback Variance (Sec.9.12.5):

- a) to vary the minimum yard setbacks in accordance with the following and as shown on Schedule 'A':
 - i. front yard: from 4.5m to 0.0m for the parkade retaining wall and building;
 - ii. side yard: from 4.5m to 0.3m and 0.0m for the parkade retaining wall;
 - iii. south side yard: from 4.5m to 1.7m for the parkade retaining wall and building;
 - iv. rear yard: from 9.0m to 0.7m and 4.4m for the parkade retaining wall;

Landscape Buffer Variance (Sec.6.6.2, Table 6.1):

- b) to vary the minimum front, side and rear landscape buffer from 1.5m to 0.0m as shown on Schedule 'A';

Parking Variance (Sec.7, Table 7.1):

- c) to vary the minimum number of required off-street parking spaces from 71 spaces to 54 spaces.

Schedule B Variance:

- d) to vary Schedule B, to reduce the required right of way for 31A Street from 18.5m to 15.8m.

Subdivision and Development Servicing Bylaw #3843

- a) to vary the off-site works (Schedule A – Level of Service) standards to asphalt widening of 0.8m, new upright concrete curb and gutter and 1.5m wide concrete sidewalk in 31A Street adjacent to the property.

3. The subject property has an area of 0.21 ha (0.54 ac). Following the adoption of Zoning (RH1 Floor Space Ratio) Text Amendment Bylaw #5661, 2017, the new density calculation method for the RH1 zoning district is as follows:

9.12.5 Development Regulations

(a) Density:

The maximum Floor Space Ratio (FSR) is 1.50, except that:

- With a housing agreement pursuant to Section 4.9, the maximum **density** shall be increased by FSR 0.25; and
- Where **parking spaces** are provided completely beneath habitable space of a primary **building** or beneath useable common amenity areas, providing that in all cases the parking spaces are screened from view, the maximum **density** shall be increased by FSR 0.25; or
- Where all the required parking is not accommodated completely beneath the habitable space of a primary **building** or useable common amenity areas, the additional density permitted shall be determined through multiplying the FSR 0.25 by the percentage of parking proposed to be provided beneath habitable space of a primary **building** or useable common amenity areas.

The FSR of the proposed 43 unit apartment building is 1.55. As illustrated in Attachments 1 and 2, 64% of the parking area is provided beneath the building. As such, the project qualifies for an additional density bousing of 0.16 FSR. The maximum permitted FSR for the project is 1.66.

4. As illustrated in Attachments 3 to 5, the proposed four and a half storey apartment building incorporates modern contemporary design, influenced by the new Vernon Jubilee Hospital tower. The first half story is used for partly covered car parking spaces, bicycle parking, a mechanical room, building manager's office and an entrance lobby. The four storeys above include 43 dwelling units and a common exercise room.
5. The proposed building location on the site is in conjunction with 31A Street future sidewalk (Attachments 1 and 2). The sidewalk would be required to be constructed on the road right of way (ROW). In order to meet the road ROW standards and provide an additional separation between the neighbouring properties at the rear, the applicant is proposing to construct the building on the back of sidewalk. Therefore, the applicant is asking for a front yard setback variance from 4.5m to 0.0m.

The applicant has also requested additional setback variances for the south side of the building and for the perimeter retaining wall in the parkade area (Attachments 1 - 3). The retaining wall is required in order to support the grade differences between the subject and adjacent properties. However, the building itself meets the minimum setback requirements in Zoning Bylaw #5000 on the north side yard (4.5m) and on the rear yard (9.0m). The side yard setback requirement on the south corner of the building, adjacent to the laneway, would also need to be reduced from 4.5m to 1.7m for the both parkade retaining wall and the building. In support of the setback variances, the applicant has provided a shadow analysis illustrating the building's shadow during different times and seasons of the year (Attachment 6).

Administration supports the requested setback variances due to the required widening of 31A Street and the grade of the site. The proposed building location would provide the minimum side and rear setback requirements on the north and east sides between the upper four floors and the adjacent proprieties. The separation between the building and the properties to the south would be achieved by the presence of 6.0m laneway.

6. The proposal does not include a landscape buffer of 1.5m on a portion of the front, side and rear yard (Attachment 7) as a result of the requested variances for building siting and parkade retaining wall. However, the proposed building design (Attachment 5) compensates the reduction of the landscape buffer. As illustrated in Attachment 7, only a portion of the front yard would not include any landscape treatment, and the applicant is proposing to use planter boxes instead. The applicant is also proposing a physical screen with wood fencing, which provides a visual separation between the lower parking area and neighbouring properties. As illustrated in Attachment 5, the building's design would provide a highly articulated façade and street presence that would reduce its visual impact. The exterior design uses structural elements and variety of material and colours, such as wood cladding, metal columns, dark bricks, horizontal groove lines and yellow highlights. The landscaping plan as proposed contributes to soften these elements and takes into consideration a long term maintenance and impact on the building structure and foundations. As such, Administration supports the requested variance.
7. The proposed apartment would provide 43 rental units comprising of 11 one bedroom, 20 two bedroom and 12 three bedroom units (Attachment 4). Zoning Bylaw #5000 requires 74 off-street parking spaces (six of which would be designated as visitor parking), at 1.72 spaces per unit (1.58 resident spaces per unit). The applicant is proposing 54 parking spaces (four of which would be designated as visitor parking), resulting in 1.26 spaces per unit (1.16 resident spaces per unit).
8. In support of the parking variance, the applicant provided a parking study dated December 20, 2017 prepared by Watt Consulting Group (Attachment 8). The parking study provides results from a survey and site observations of seven existing apartment sites to estimate the number of vehicles owned per

unit. The average vehicle ownership was 0.89 vehicles per unit, while the 85th percentile ownership was 1.15 vehicles per unit. Overall, it is anticipated the resident parking demand for the site will be 1.15 vehicles per unit (49 parking spaces for residents), suggesting that the proposed 1.16 resident spaces per unit would meet the anticipated parking demand for the development site.

9. Visitor parking demand was also reviewed at the existing apartment sites, and it was found that there was an average demand rate of 0.10 spaces per unit. For this development, that means four parking spaces should be designated as visitors parking. The total parking demand (visitors and residents) would total 53 parking spaces.
10. In order to further support the parking reduction, the applicant would implement a car share program at the site, utilizing the Okanagan Car Share Co-op (OGO). The development site would be the first exposure of the OGO program within Vernon. In an agreement between the applicant and OGO dated January 17, 2018 (Attachment 9), the applicant agreed to purchase a vehicle, at a cost of approximately \$34,000, which would be located on-site and would be available to all OGO members. The applicant would also purchase OGO memberships for all two bedroom and three bedroom units in the development to encourage more use of the program. As quoted in the parking study (Attachment 8), car sharing programs contribute to reduced vehicle ownership among members by approximately 50%. Moreover, other jurisdictions' zoning bylaws allow for fewer parking spaces per car share vehicle provided on a development. For example, Penticton's Zoning Bylaw supports a reduction of six parking spaces if the development provides a car share vehicle and a car share parking space. As per the parking study, it was estimated that providing the car share program at the development site would reduce the parking space demand by four parking spaces. This means the overall demand would be reduced from 53 parking spaces to 49 parking spaces. Therefore, the proposed 54 parking spaces would exceed the estimated demand for the development.
11. The BC Building Code requires a minimum of one of the parking spaces to be sized and signed for handicapped parking. The applicant is proposing two handicapped parking spaces. There are to be bicycle parking areas in the parkade as well as the proposed car share program.
12. 31A Street adjacent to this property is identified in the Master Transportation Plan MTP as a Local Road with sidewalk on both sides. The road ROW width adjacent to the property varies from 14.7m wide to 12.2m wide. The 14.7m wide section resulted from a previous 2.5m wide road dedication adjacent to Lot 1, Plan KAP91099 (hospital parking lot) was provided on the west side of 31A Street. The remainder of 31A Street adjacent to the property (approximately 4m) has a road ROW width of 12.2m (40'). Based on the original road centerline a setback of 3.15m would apply to new construction, as per Schedule B of Zoning Bylaw #5000. This was to enable an ultimate 18.5m wide road ROW consistent with the SDSB #3843 road ROW width adjacent to this zoning designation which specified a 12m wide asphalt road surface.
13. Subdivision and Development Servicing Bylaw (SDSB) #3843, Schedule A requires an asphalt width of 12m with 1.5m wide sidewalk on both sides of 31A Street. This standard was intended to provide adequate width for two 3.5m wide travel lanes and two 2.5m wide parking lanes. Based on ITF lane travel lane and parking lane widths of 3.25m and 2.4m respectively, the total asphalt width required to provide the same function would be 11.3m. Administration therefore supports a variance to SDSB #3843, Schedule A to reduce the total pavement width from 12m to 11.3m, as shown in Table 1. This will result in the applicant widening the existing asphalt by 0.8m and installing new upright concrete curb and gutter and a 1.5m wide concrete sidewalk adjacent to their property. In order to accommodate the on street works adjacent to the full frontage of the property, additional road dedication of 2.1m would be required.
14. The supported reduced asphalt width in 31A Street adjacent to this property enables a reduction of the road ROW width necessary to support those works. Zoning Bylaw #5000 Schedule B identifies a setback based on an ultimate 18.5m road ROW width at this location. Support for the reduced asphalt width

enables a reduction of the road ROW width necessary. Based on the original road centerline, Administration supports the requested variance to Zoning Bylaw #5000, Schedule B to reduce the setback adjacent to 31A Street from 18.5m to 15.8m, as shown in Table 1. This width provides space behind sidewalk for streetlights and shallow utilities. Based on the original road centerline, this would result in a setback of 1.8m for new construction.

Table 1

31A Street	ROW Width	Asphalt Width	Concrete Curb & Gutter	Sidewalk	Ornamental Streetlights
Existing	12.2m-14.7m	8.5m-10.5m	Yes	None	None (on utility poles on opposite side)
Bylaw#3843	18.5m	12.0m	Yes	1.5m wide	Yes
MTP	NA	NA	NA	Required	NA
ITF	15.3m	11.3m	Yes	2.0m wide	One side
Variance Supported	15.8m	11.3m	Yes	1.5m	To be determined

15. Should Council support DVP00418, Administration recommends that the following conditions be required as part of DVP approval:

- a) The owner is to dedicate road right-of-way widening of approximately 2.1m on Lot 1, Plan 21336, Sec 34, Twp 9, ODYD (1803 – 31A Street); and
- b) That the site, floor, elevations, landscaping plans, parking study and the Schedule 'A' noted as Attachments 1 to 8 to be attached to and form part of DVP00418 as Schedule 'A'.

C. Attachments:

- Attachment 1 – Schedule 'A' (yard setbacks and landscaping buffer variances location)
- Attachment 2 – Proposed site plan
- Attachment 3 – Main floor and parkade plan
- Attachment 4 – Proposed 2nd to 5th floor and roof plan
- Attachment 5 – Elevations
- Attachment 6 – Shadow study
- Attachment 7 – Landscape plan
- Attachment 8 – Parking study
- Attachment 9 – Car share program agreement

D. Council's Strategic Plan 2015 – 2018 Goals/Deliverables:

The subject application involves the following deliverable in Council's Strategic Plan 2015 – 2018:

- Support sustainable neighbourhoods by implementing neighbourhood plans and the OCP.
- Support sustainable urban development by reducing off site costs and reviewing parking requirements for infill development

E. Relevant Policy/Bylaws/Resolutions:

1. The subject property is designated Mixed Use – High Density Commercial and Residential on the land use map in the Official Community Plan.

2. The property is zoned RH1 – Low Rise Apartment Residential as per Zoning Bylaw #5000.
3. The *Local Government Act* provides Council with the authority to vary local bylaws based on site specific considerations. The granting of such variances does not set a precedent within the community for future variances to be based upon, as each variance application must be evaluated on its own merit and potential implications to the whole community and the specific neighbourhood.


BUDGET/RESOURCE IMPLICATIONS:

The applicant has reviewed the City’s Rental Housing Incentive Grant program and associated criteria. As the project is for a private sector purpose built 43 rental unit apartment, there may be a grant application submitted if the project is approved. The program allows for a grant amount of up to \$4,000 per unit, with the approval being subject to a number of variables such as funding availability, the project design meeting SAFERhome standards and the adoption and registration of a Housing Agreement Bylaw and covenant.

Prepared by:

Approved for submission to Council:

Right-click to sign
with **DocuSign**



Will Pearce, CAO

Roy Nuriel
Economic Development Planner

Date: _____

Right-click to sign
with **DocuSign**

Kim Flick
Director, Community Infrastructure and Development

REVIEWED WITH

- | | | |
|--|---|--|
| <input type="checkbox"/> Corporate Services | <input type="checkbox"/> Operations | <input checked="" type="checkbox"/> Current Planning |
| <input type="checkbox"/> Bylaw Compliance | <input type="checkbox"/> Public Works/Airport | <input type="checkbox"/> Long Range Planning & Sustainability |
| <input type="checkbox"/> Real Estate | <input type="checkbox"/> Facilities | <input type="checkbox"/> Building & Licensing |
| <input type="checkbox"/> RCMP | <input type="checkbox"/> Utilities | <input checked="" type="checkbox"/> Engineering Development Services |
| <input type="checkbox"/> Fire & Rescue Services | <input type="checkbox"/> Recreation Services | <input type="checkbox"/> Infrastructure Management |
| <input type="checkbox"/> Human Resources | <input type="checkbox"/> Parks | <input checked="" type="checkbox"/> Transportation |
| <input type="checkbox"/> Financial Services | | <input type="checkbox"/> Economic Development & Tourism |
| <input checked="" type="checkbox"/> COMMITTEE: APC (Feb.14/18) | | |
| <input type="checkbox"/> OTHER: | | |

Schedule 'A'

a (i). to vary the minimum front yard setback from 4.5m to 0.0m for the parkade retaining wall and building (Section 9.12.5);
b. to vary the minimum front landscape buffer from 1.5m to 0.0m (Section 6.6.2, Table 6.1).

a (ii). to vary the minimum side yard setback from 4.5m to 0.0m for the parkade retaining wall (Section 9.12.5);
b. to vary the minimum side landscape buffer from 1.5m to 0.0m (Section 6.6.2, Table 6.1).

a (iv). to vary the minimum rear yard setback from 9.0m to 0.7m for the parkade retaining wall (Section 9.12.5);
b. to vary the minimum rear landscape buffer from 1.5m to 0.0m (Section 6.6.2, Table 6.1).

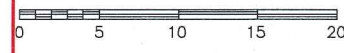
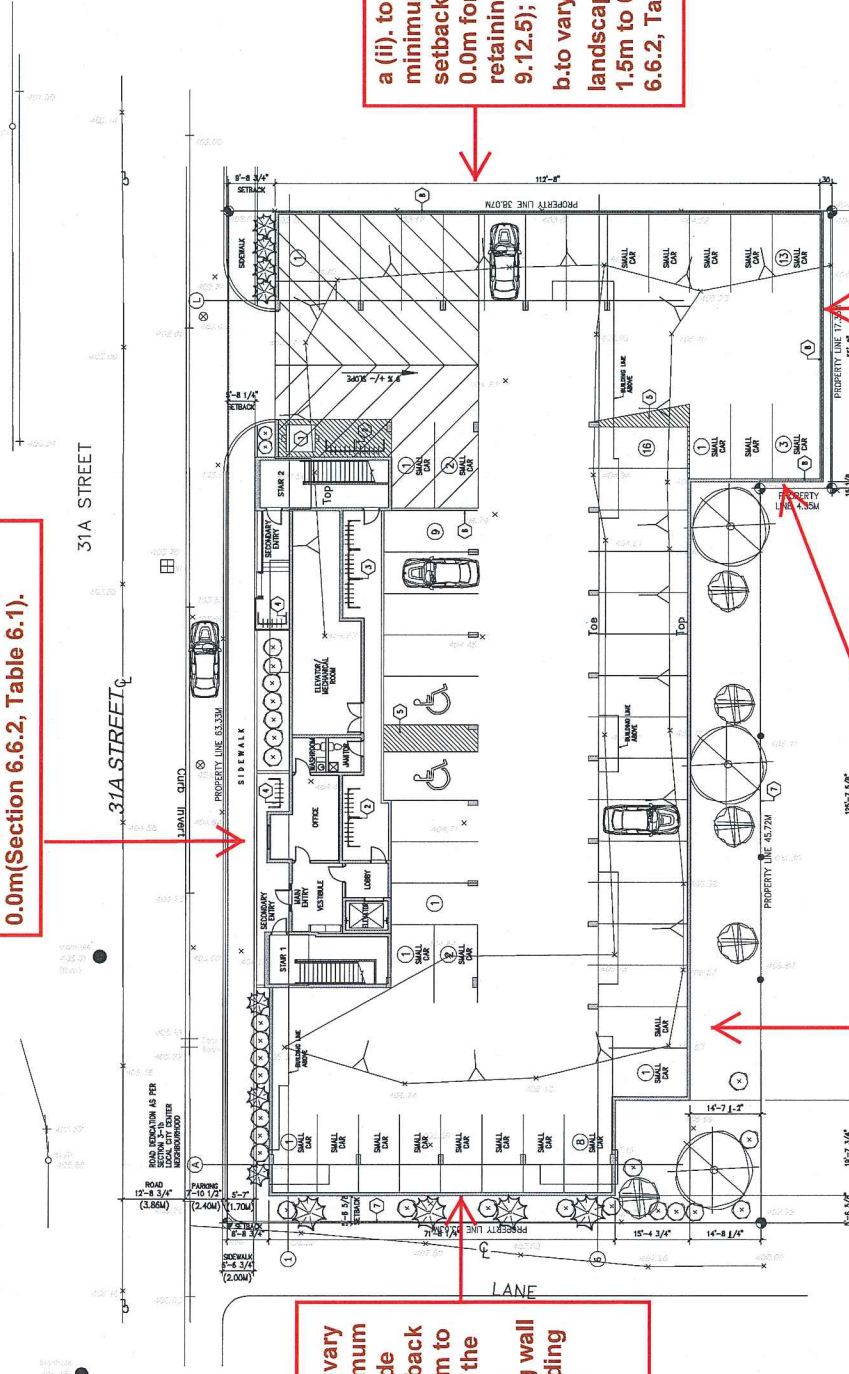
a (ii). to vary the minimum side yard setback from 4.5m to 0.3m for the parkade retaining wall (Section 9.12.5);
b. to vary the minimum side landscape buffer from 1.5m to 0.0m (Section 6.6.2, Table 6.1).

a (iv). to vary the minimum rear yard setback from 9.0m to 4.4m for the parkade retaining wall (Section 9.12.5).

a (iii). to vary the minimum south side yard setback from 4.5m to 1.7m for the parkade retaining wall and building (Section 9.12.5).

LEGEND:

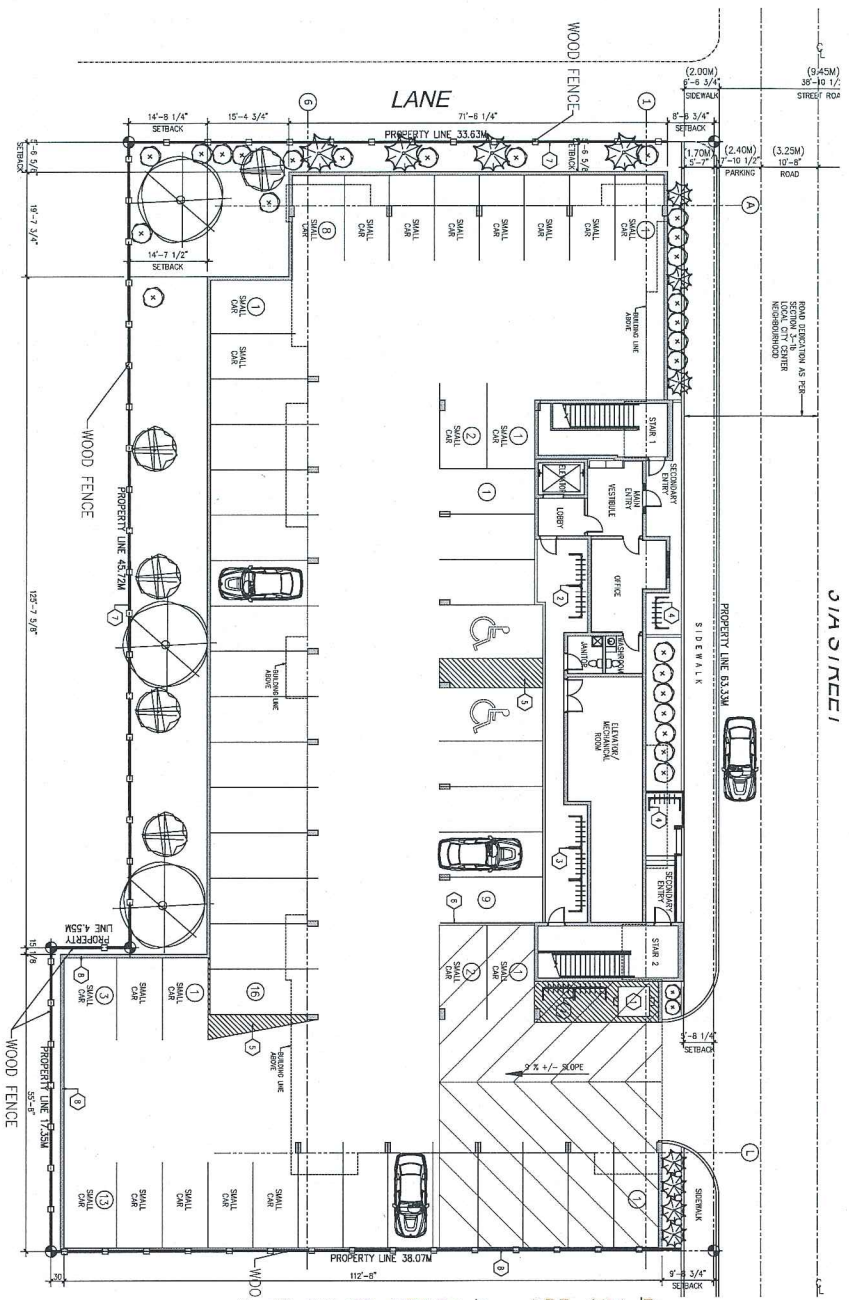
- x denotes spot elevation
- denotes iron pin found
- ⊕ denotes water valve
- ⊕ denotes utility pole
- ⊗ denotes "No Parking" sign



SITE PLAN OF LOT 1, PLAN 21336,
SEC 34, TP 9, ODYD

SCALE: 1 : 300	OUR FILE: R10268
DATE: 20 Sep. 2017	DRAWN: RL
Ironclad Developments Inc.	
WILLIAM E. MADDOX	
B.C. LAND SURVEYOR	
3500 - 30th STREET	
VERNON, BC V1T 5E8	
TELEPHONE: (250) 542-4343	

102680700



NOTE:
 6'-6" HIGH WOOD FENCE
 TYP. 4'-0" PANELS
 STEPPED DOWN TO FOLLOW FINAL GRADE
 6'-6" TO BE MEASURED AT HIGHEST ELEVATION

DEVELOPMENT DESCRIPTION:
 1803 31A STREET, VERNON DEVELOPMENT
 SITE AREA: +/- 2,03.85 m²
 CURRENT ZONING: R1H

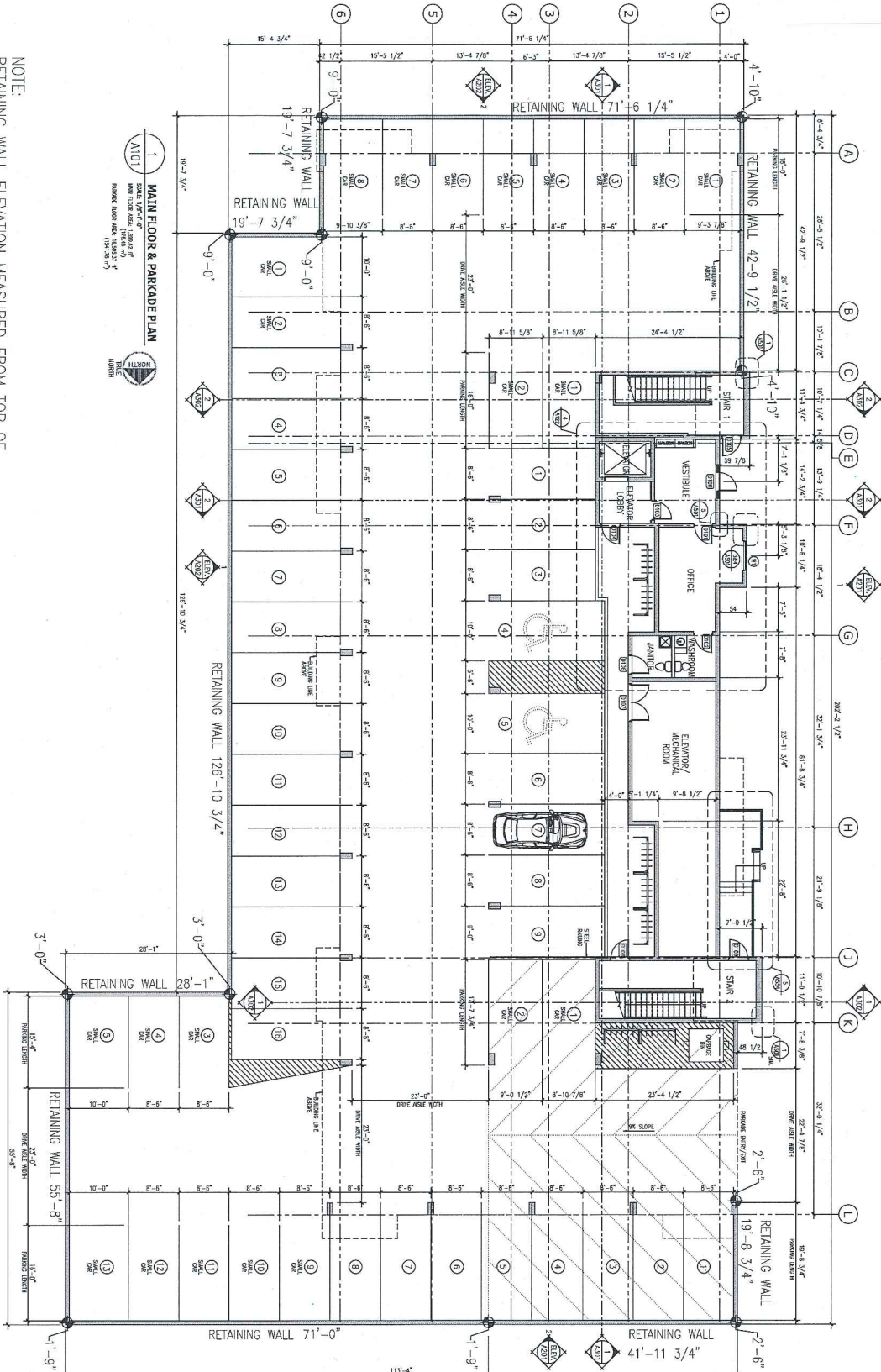
PERMITTED UNITS = 43 UNITS
 PROPOSED UNITS: 5 BEDROOM: 11
 2 BEDROOM: 24
 3 BEDROOM: 8

PARKING
 Total Parking Area: 1,440.27 m²
 Covered Parking: 822 m²
 Uncovered Parking Total Area: 618.65 m²
 Internal Landscaping Total Area: 364.37 m²

REQUIRED PARKING: 70.5
 (INCLUDING VISITOR STALLS)
 PROPOSED PARKING: 54
 (INCLUDING VISITOR STALL)
 54 SURFACE STALLS
 TOTAL=54 PARKING STALLS
 PROPOSED BICYCLE PARKING = 45 STALLS

		Project Management General Contracting Energy Build
101-57188 Symington Rd. Springfield, NB R2L 4L5		Piv. 204-777-1972 info@ironclad.ca
IRONCLAD DEVELOPMENTS INC.		
Proposed Site Plan 1/2 The Highland Apartments 1803 31A Street Vernon, British Columbia		
Project Number 1704	Date November 24, 2017	Rev. Date None
Drawn By II	Checked By RG	Sheet No. A001
Scale AS NOTED		

NOTE:
RETAINING WALL ELEVATION MEASURED FROM TOP OF
THE CONCRETE SLAB.



Project Number: 1704
 Date: November 24, 2017
 Drawn By: T/JT
 Checked By: RG
 Scale: AS NOTED

MAIN FLOOR & PARKADE PLAN

The Highland Apartments
 1803 31A Street
 Vernon, British Columbia

TRONCLAD DEVELOPMENTS INC.
 101-57158
 Synnington Rd.
 Springfield, NB
 R2J 4L6
 Ph: 204-777-1972
 info@trond.ca

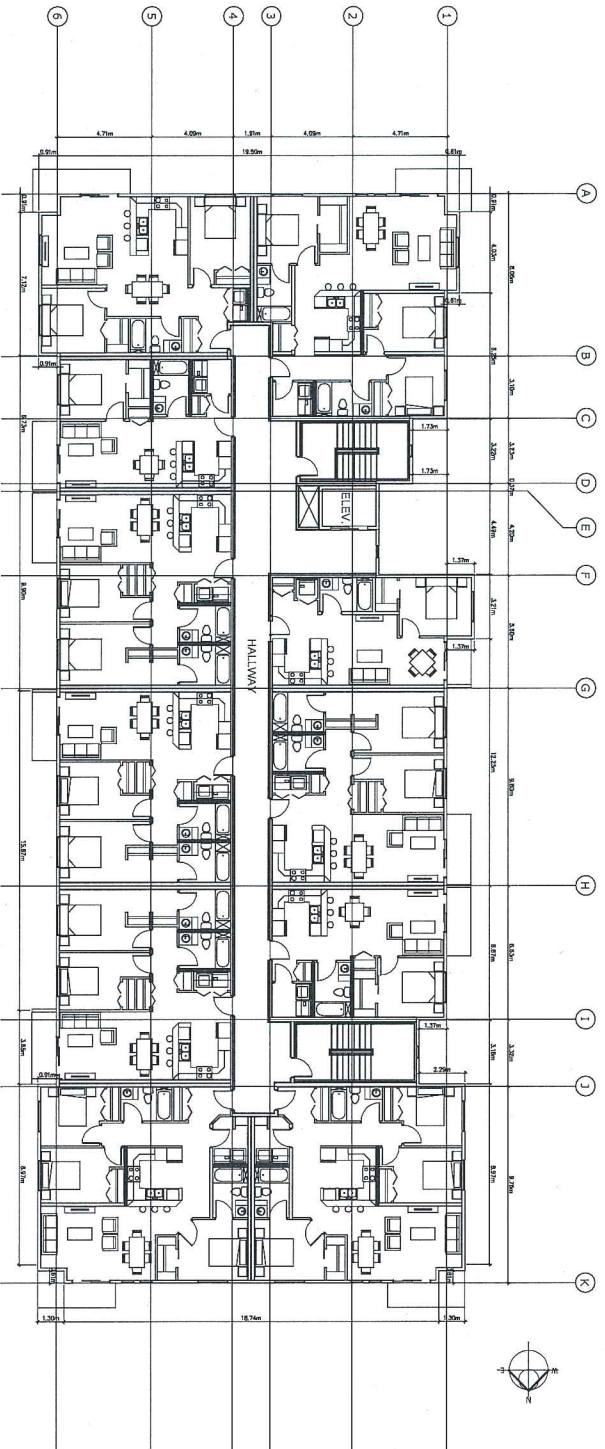
NO.	REVISION DESCRIPTION	DATE
1	ISSUED FOR FOUNDATION PERMIT	2017/12/21

THIS DRAWING IS THE PROPERTY OF TRONCLAD DEVELOPMENTS INC. AND SHALL BE KEPT IN CONFIDENCE. IT IS TO BE USED ONLY FOR THE PROJECT AND SITE SPECIFICALLY IDENTIFIED HEREON. IT IS NOT TO BE REPRODUCED, COPIED, REPRODUCED, OR TRANSMITTED IN ANY FORM OR BY ANY MEANS, ELECTRONIC OR MECHANICAL, INCLUDING PHOTOCOPYING, RECORDING, OR BY ANY INFORMATION STORAGE AND RETRIEVAL SYSTEM, WITHOUT THE WRITTEN PERMISSION OF TRONCLAD DEVELOPMENTS INC.

TRONCLAD DEVELOPMENTS INC.

Project Management
 General Contracting
 Structural Detail

AS NOTED



PROPOSED TYP 3rd & 4th FLOOR PLAN

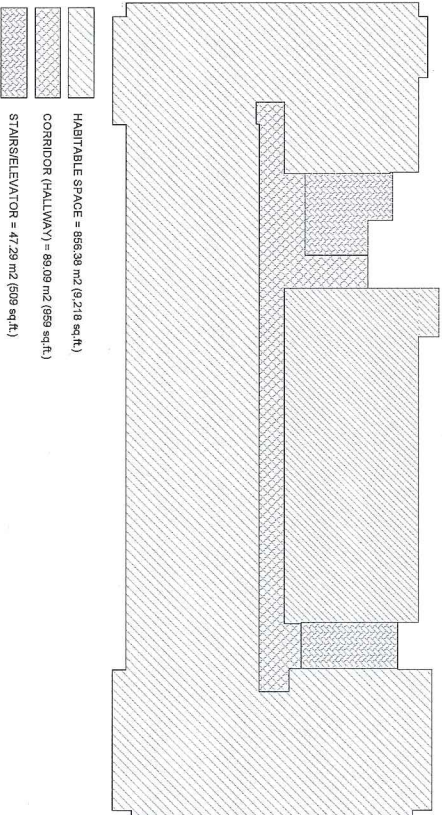
SCALE

NTS

AREA

1,061 m² (11,424 sq.ft.)

FLOOR AREA BREAKDOWN



- HABITABLE SPACE = 695.39 m² (9,218 sq.ft.)
- CORRIDOR (HALLWAY) = 89.09 m² (959 sq.ft.)
- STAIRS/ELEVATOR = 47.29 m² (509 sq.ft.)

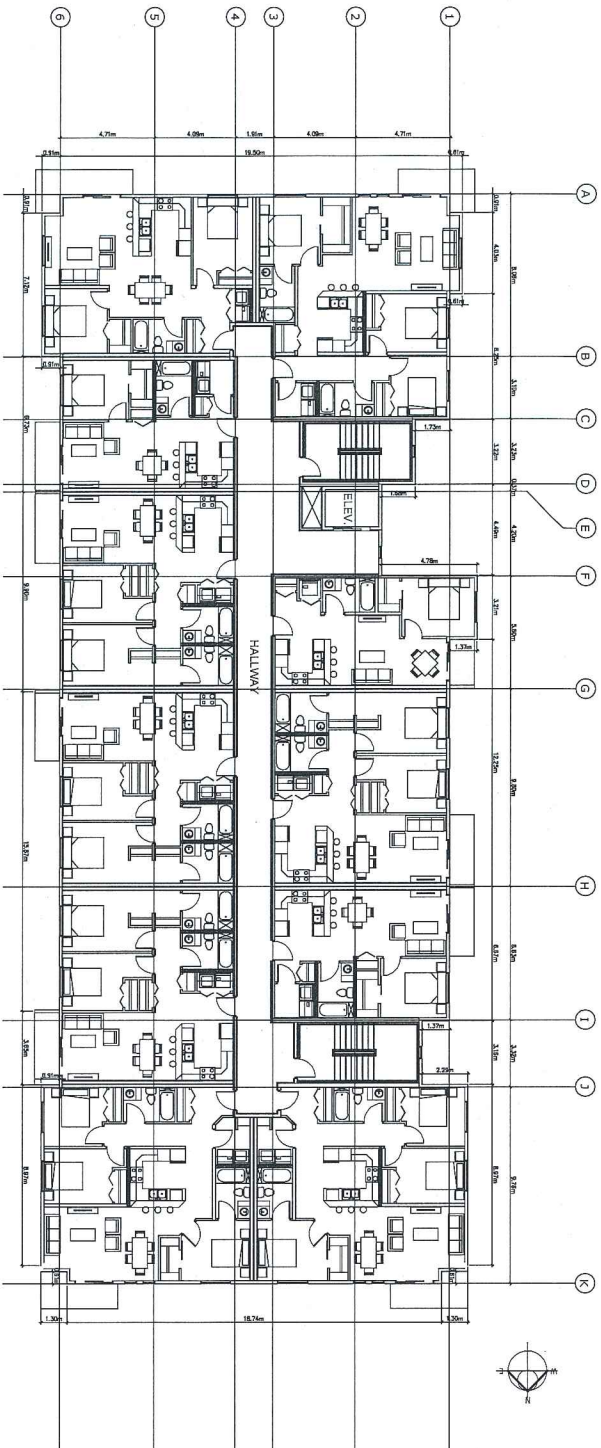


IRONCLAD
CORPORATION

PROJECT
Proposed Apartment Development
1803 31A STREET
1803 31A Street, Vernon, BC

DATE : November 06, 2017
FILE : 1704-Vernon II
SHEET NO.
PD. 2.2

Tel: 250.477.9772 info@ironclad.ca
COPYRIGHT © 2016 - 2017



PROPOSED FIFTH FLOOR PLAN

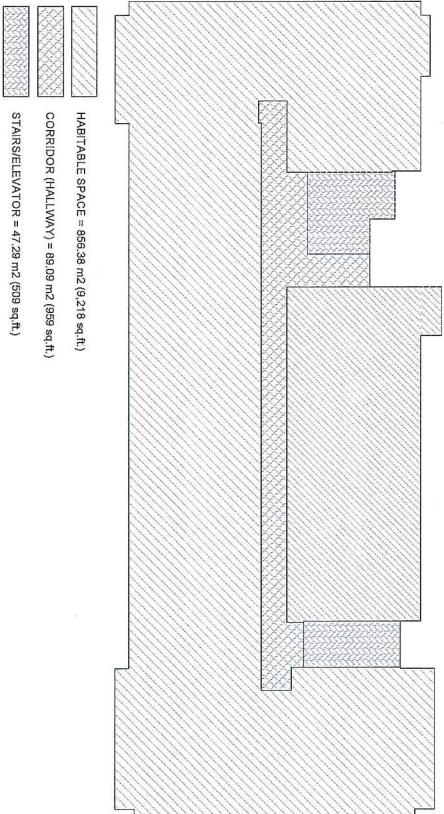
SCALE

NTS

AREA

1,061 m² (11,424 sq.ft.)

FLOOR AREA BREAKDOWN



IRONCLAD
DEVELOPMENT INC.

PROJECT
Proposed Apartment Development
1803 31A STREET
1803 31A Street, Vernon, BC

DATE : November 06, 2017
FILE : 1704-Vernon II
SHEET No.
PD. 2.3

Tel 2 0 4 7 7 1 9 7 2 info@icdev.ca
COPYRIGHT © 2015 - 2017

HABITABLE SPACE = 855.58 m² (9,218 sq.ft.)

CORRIDOR (HALLWAY) = 85.09 m² (919 sq.ft.)

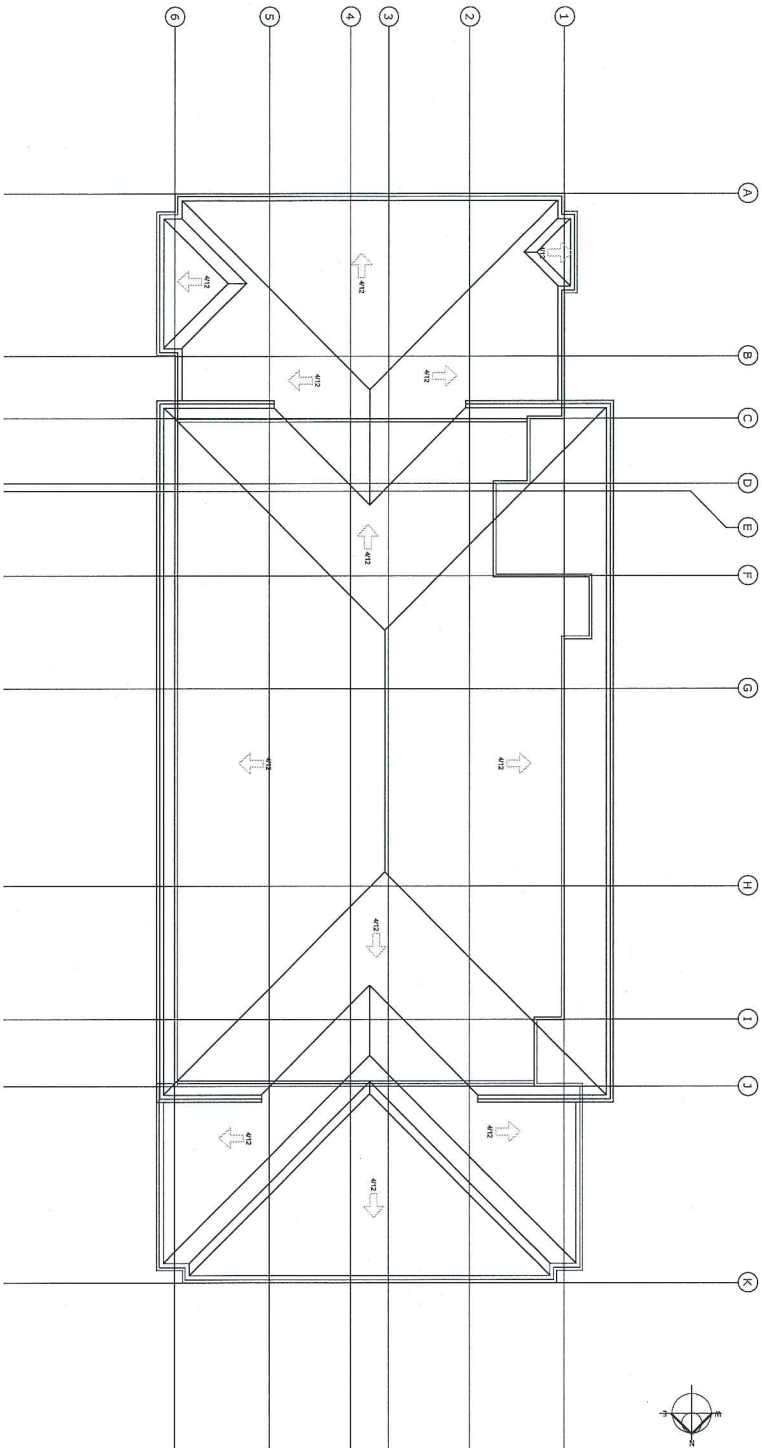
STAIR/ELEVATOR = 47.29 m² (509 sq.ft.)



IRONCLAD
 CONSULTING ENGINEERS INCORPORATED
 TEL 2 0 4 7 7 1 9 7 2 info@ironclad.ca
 COPYRIGHT © 2010 - 2017

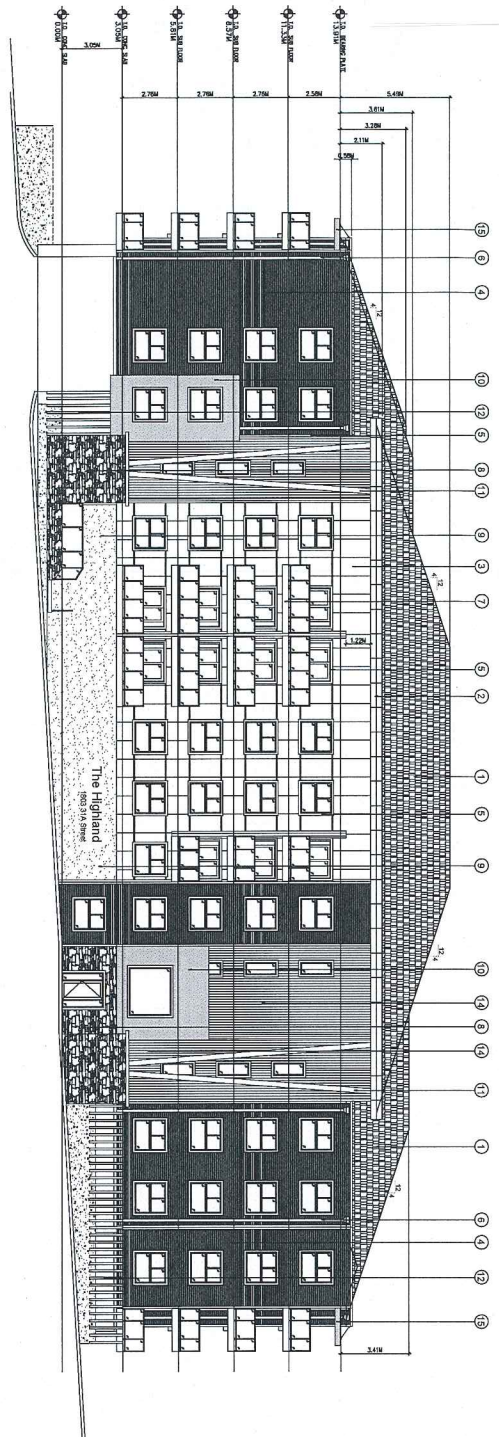
PROJECT
Proposed Apartment Development
1803 31A STREET
 1803 31A Street, Vernon, BC

DATE : November 06, 2017
 FILE : 1704-Vernon II
 SHEET No. **PD. 2.4**



PROPOSED ROOF PLAN

SCALE
 NTS
 AREA
 N/A



- 1 ASPHALT SHINGLES (MIDNIGHT BLACK)
- 2 PREFIN. ALUMINUM CLADDING (MILL FINISH)
- 3 HARDIE PANEL (ARCTIC WHITE, SMOOTH W/ BLACK EZ LOCK PROFILE, EASY TRIM ON HORIZONTAL ONLY)
- 4 CORRUGATED METAL SHEET (GRAY)
- 5 4" HARDIE TRIMS (IRON GRAY)
- 6 PRE-FABRICATED METAL/L. SCUPPERS W/ DOWN SPOUTS (COLOR MATCHED)
- 7 12" HARDIE BOARD (IRON GRAY)
- 8 THIN MASONRY BRICK FROM ALSIPS (BLACK DIAMOND) OR EQUIVALENT
- 9 PAINTED CONCRETE (FLAT BLACK)
- 10 ACRYLIC STUCCO (CLOVERDALE - YELLOW #AC 109)
- 11 10" Ø ROLLED STEEL (MILL FINISH)
- 12 FRAMED 2x4 SCREENING CLAD W/ BROWN TREATED LUMBER OR SIMILAR (SIZE TO BE DETERMINED)
- 13 BROWN TREATED FENCING
- 14 LUX ARCHITECTURAL PANEL (CEDAR)
- 15 PRE-FIN. AL. CLADDING (CLOVERDALE - AC109)



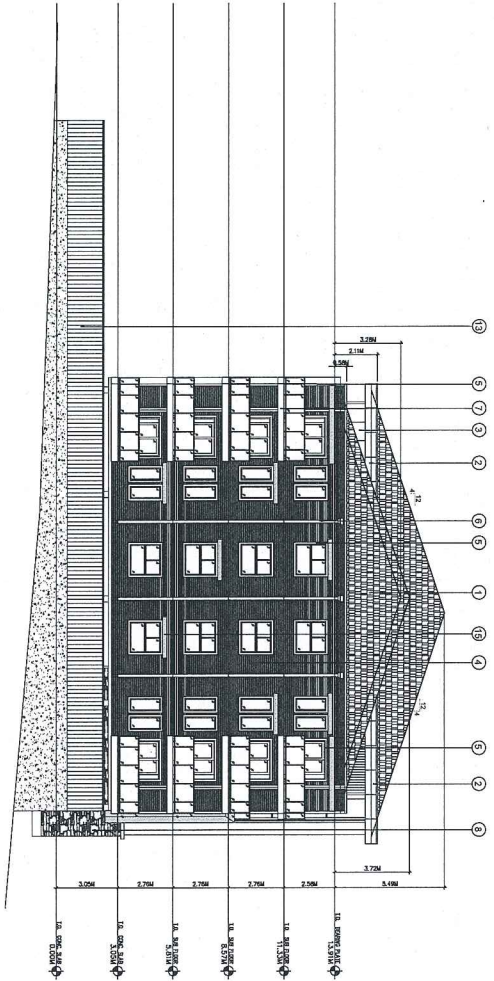
IRONCLAD
DEVELOPMENT INC.
PROJECT
Proposed Apartment Development
1803 31A STREET
1803 31A Street, Vernon, BC

DATE : February 06, 2018
FILE : 1704-Vernon II
SHEET NO.
pd. 3.0

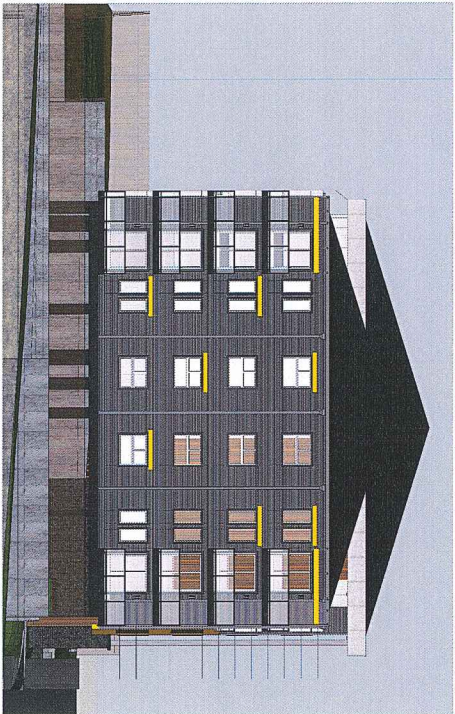
Tel: 250.477.7197, info@ironclad.ca
Copyright © 2018 - 2017

PROPOSED WEST ELEVATION

SCALE
NTS



- ① ASPHALT SHINGLES (MIDNIGHT BLACK)
- ② PREFIN. ALUMINUM CLADDING (MILL FINISH)
- ③ HARDIE PANEL (ARCTIC WHITE, SMOOTH W/ BLACK EZ LOCK PROFILE, EASY TRIM ON HORIZONTAL ONLY)
- ④ CORRUGATED METAL SHEET (GRAY)
- ⑤ 4" HARDIE TRIMS (IRON GRAY)
- ⑥ PRE-FABRICATED METAL SCUPPERS W/ DOWN SPOUTS (COLOR MATCHED)
- ⑦ 12" HARDIE BOARD (IRON GRAY)
- ⑧ THIN MASONRY BRICK FROM ALSIPS (BLACK DIAMOND) OR EQUIVALENT
- ⑨ PAINTED CONCRETE (FLAT BLACK)
- ⑩ ACRYLIC STUCCO (CLOVERDALE - YELLOW #AC-109)
- ⑪ 10" Ø ROLLED STEEL (MILL FINISH)
- ⑫ FRAMED 2X4 SCREENING CLAD W/ BROWN TREATED LUMBER OR SIMILAR (SIZE TO BE DETERMINED)
- ⑬ BROWN TREATED FENCING
- ⑭ LUX ARCHITECTURAL PANEL (CEDAR)
- ⑮ PRE-FIN. AL. CLADDING (CLOVERDALE - AC109)



IRONCLAD
 INDUSTRIAL PARKWAY 1803
 VERNON, BC V1P 6K1

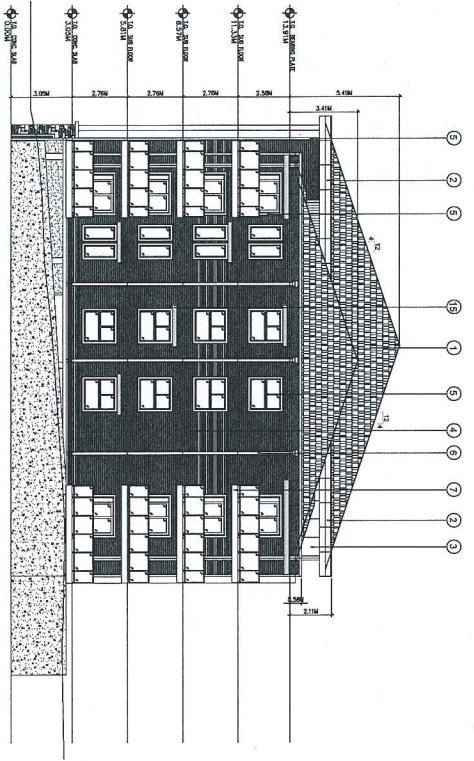
Tel 2 0 4 7 7 1 9 7 2 info@icdev.ca
 COPYRIGHT © 2010 - 2017

PROJECT
Proposed Apartment Development
1803 31A STREET
 1803 31A Street, Vernon, BC

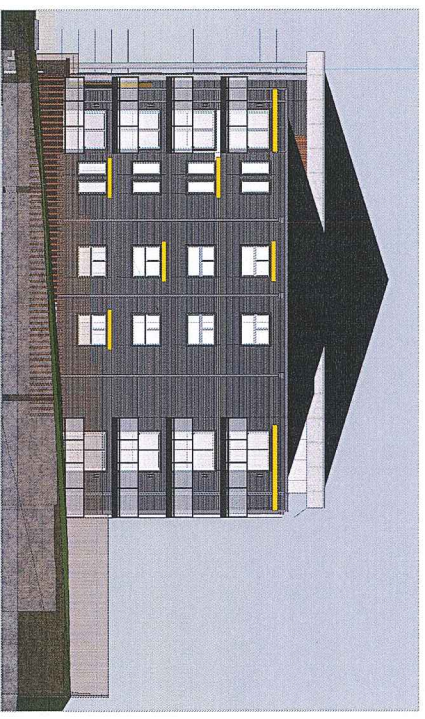
DATE : November 06, 2017
 FILE : 1704-Vernon II
 SHEET No.
PD. 3.1

PROPOSED NORTH ELEVATION
 SCALE

N.T.S



- ① ASPHALT SHINGLES (MIDNIGHT BLACK)
- ② PREFIN. ALUMINUM CLADDING (MILL FINISH)
- ③ HARDIE PANEL (ARCTIC WHITE, SMOOTH W/ BLACK EZ LOCK PROFILE, EASY TRIM ON HORIZONTAL ONLY)
- ④ CORRUGATED METAL SHEET (GRAY)
- ⑤ 4" HARDIE TRIMS (IRON GRAY)
- ⑥ PRE-FABRICATED METAL. SCUPPERS W/ DOWN SPOUTS (COLOR MATCHED)
- ⑦ 12" HARDIE BOARD (IRON GRAY)
- ⑧ THIN MASONRY BRICK FROM ALSIPS (BLACK DIAMOND) OR EQUIVALENT
- ⑨ PAINTED CONCRETE (FLAT BLACK)
- ⑩ ACRYLIC STUCCO (CLOVERDALE - YELLOW #AC 109)
- ⑪ 10" Ø ROLLED STEEL (MILL FINISH)
- ⑫ FRAMED 2X4 SCREENING CLAD W/ BROWN TREATED LUMBER OR SIMILAR (SIZE TO BE DETERMINED)
- ⑬ BROWN TREATED FENCING
- ⑭ LUX ARCHITECTURAL PANEL, (CEDAR)
- ⑮ PRE-FIN. AL CLADDING (CLOVERDALE - AC109)



PROPOSED SOUTH ELEVATION

SCALE

N/S

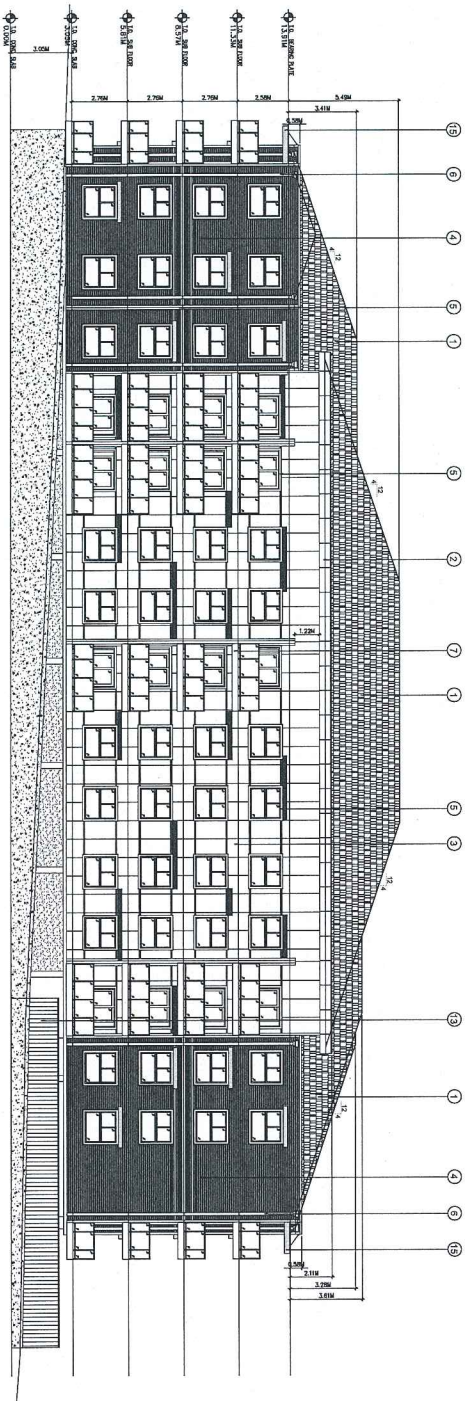


IRONCLAD
 INDUSTRIAL DEVELOPMENT INC.

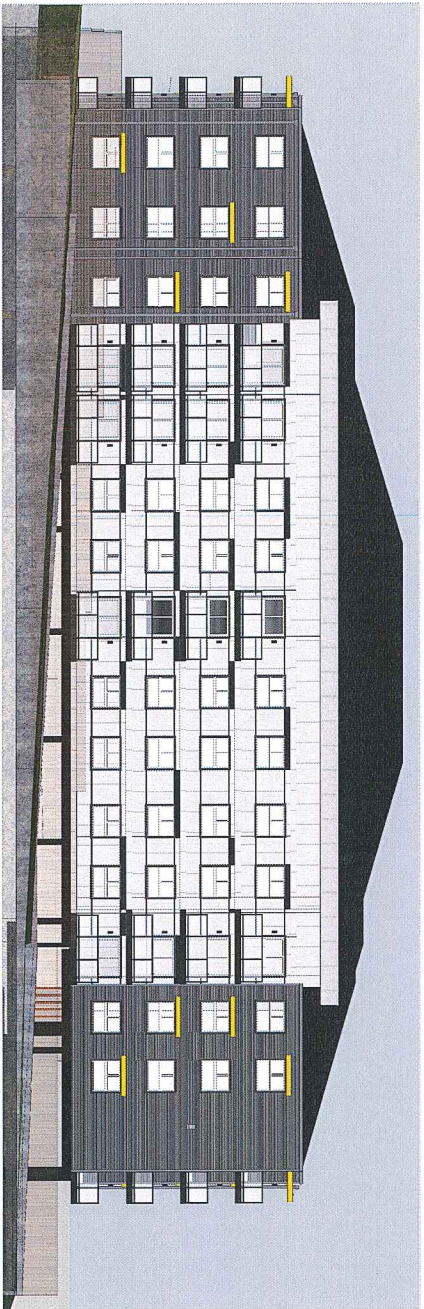
PROJECT
Proposed Apartment Development
1803 31A STREET
 1803 31A Street, Vernon, BC

DATE : November 06, 2017
 FILE : 1704-Vernon II
 SHEET NO.
PD. 3.2

Tel: 250.477.1972 info@icdev.ca
 COPYRIGHT © 2010 - 2017



- ① ASPHALT SHINGLES (MIDNIGHT BLACK)
- ② PREFIN. ALUMINUM CLADDING (MILL FINISH)
- ③ HARDIE PANEL (ARCTIC WHITE, SMOOTH W/ BLACK EZ LOOK PROFILE, EASY TRIM ON HORIZONTAL ONLY)
- ④ CORRUGATED METAL SHEET (GRAY)
- ⑤ 4" HARDIE TRIMS (IRON GRAY)
- ⑥ PRE-FABRICATED METAL/L. SCUPPERS W/ DOWN SPOUTS (COLOR MATCHED)
- ⑦ 12" HARDIE BOARD (IRON GRAY)
- ⑧ THIN MASONRY BRICK FROM ALSIPS (BLACK DIAMOND) OR EQUIVALENT
- ⑨ PAINTED CONCRETE (FLAT BLACK)
- ⑩ ACRYLIC STUCCO (CLOVERDALE - YELLOW #AC 109)
- ⑪ 10" Ø ROLLED STEEL (MILL FINISH)
- ⑫ FRAMED 2x4 SCREENING CLAD W/ BROWN TREATED LUMBER OR SIMILAR (SIZE TO BE DETERMINED)
- ⑬ BROWN TREATED FENCING
- ⑭ LIX ARCHITECTURAL PANEL (CEDAR)
- ⑮ PRE-FIN. AL CLADDING (CLOVERDALE - AC109)



PROPOSED EAST ELEVATION

SCALE

NIS



IRONCLAD
DESIGN/CONSTRUCTION INC.

PROJECT
Proposed Apartment Development
1803 31A STREET
1803 31A Street, Vernon, BC

DATE : February 06, 2018
FILE : 1704-Vernon II
SHEET No.
PD. 3.3

Tel 2 0 4 7 7 1 5 7 2 Info@idc.ca
Copyright © 2010 - 2017



PERSPECTIVE VIEWS

SCALE

NTS

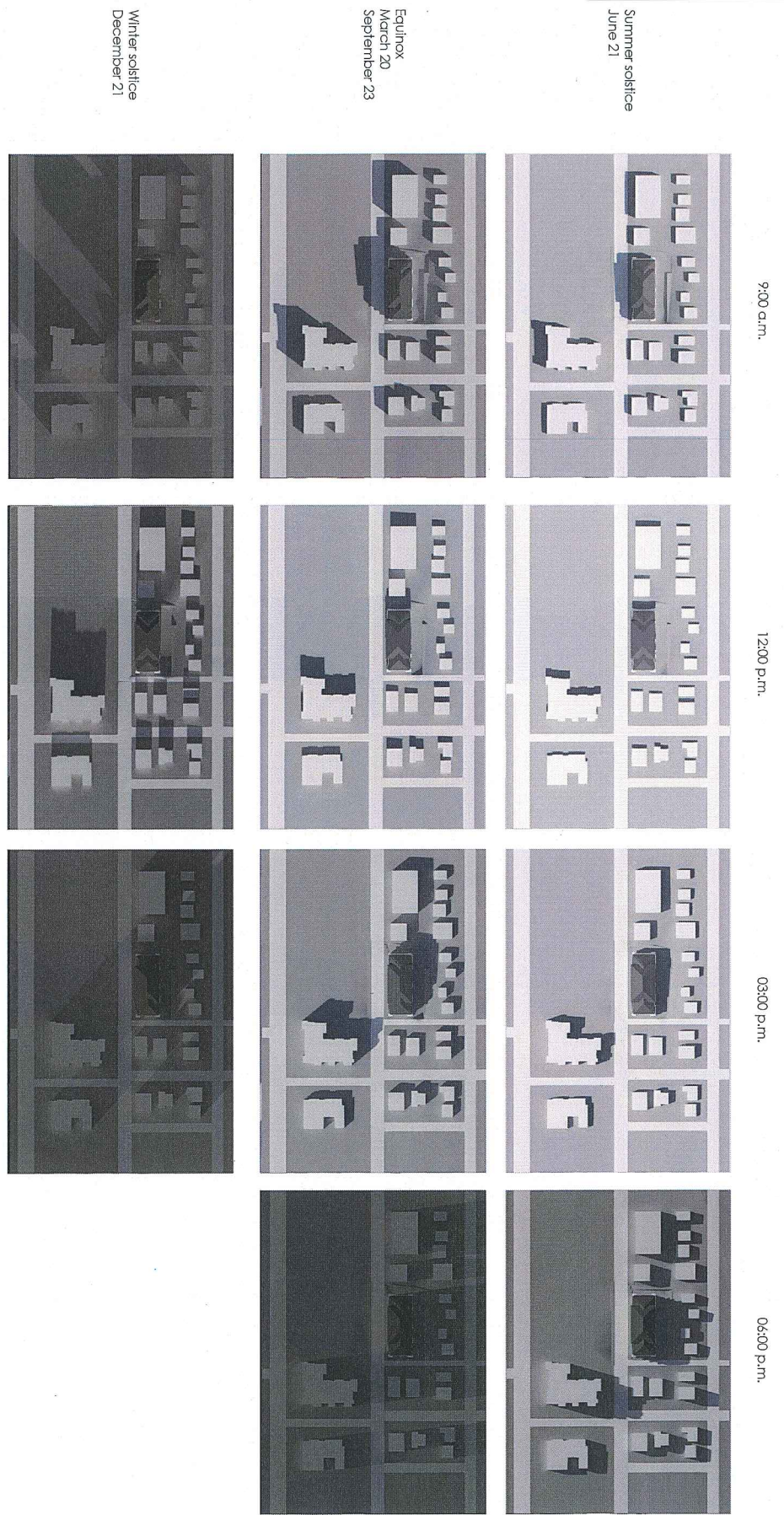


IRONCLAD
CONSTRUCTION INC.

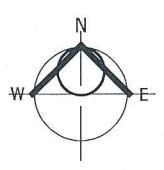
PROJECT
Proposed Apartment Development
1803 31A STREET
1803 31A Street, Vernon, BC

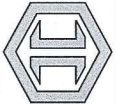
DATE : February 06, 2016
FILE : 1704-Vernon II
SHEET No.
PD. 4.0

Tel: 250.477.1197 info@icdev.ca
COPYRIGHT © 2010 - 2017

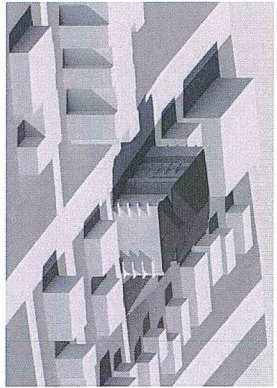


01
AD1.0
 NTS
SHADOW STUDY- PLAN VIEW

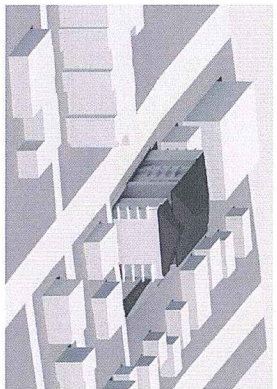


 IRONCLAD DEVELOPMENT'S INC. 101-5759 Symington Road Springfield, VA 22154 Ph: 204-777-1972 info@ironclad.com	1803 31A STREET	Drawn By	O.S.	Project No.	1704	Sheet No. AD1.0
	SHADOW STUDY PLAN VIEW	Checked By	R.G.	Date	11/01/2017	

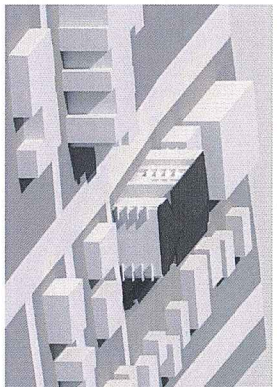
9:00 a.m.



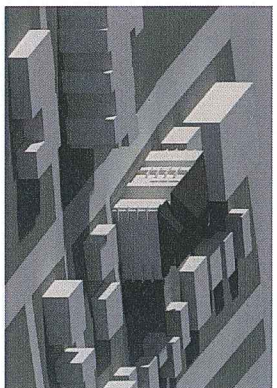
12:00 p.m.



03:00 p.m.

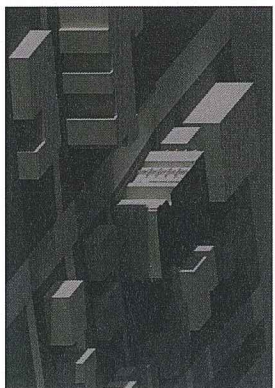
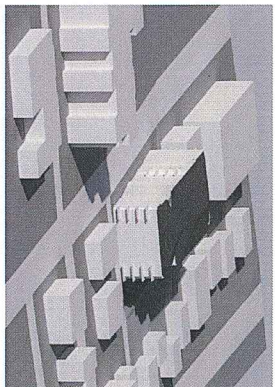
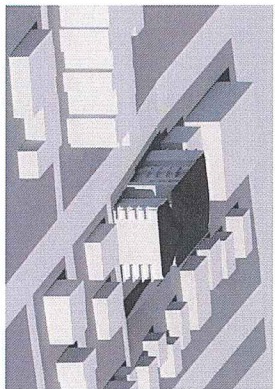
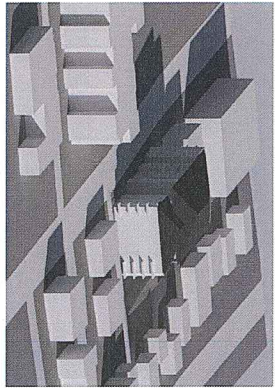


06:00 p.m.

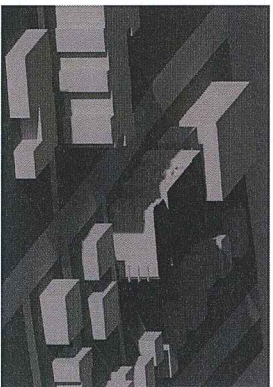
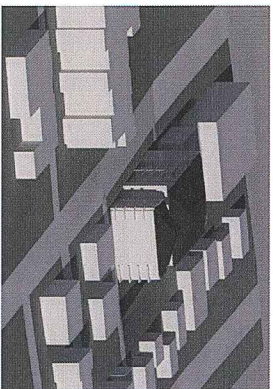
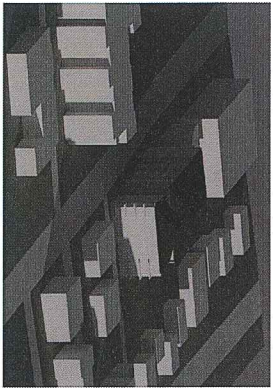


Summer solstice
June 21

Equinox
March 20
September 23



Winter solstice
December 21



01
AD1.1

SHADOW STUDY - ANGLED VIEW
NTS



IRONCLAD
DESIGN/ARCHITECTURE INC.
101-57189 Springton Road
Springfield, MA 01104
PH: 204-777-1972
info@ironclad.ca

1803 31A STREET
VERNON, BC
SHADOW STUDY
ANGLED VIEW

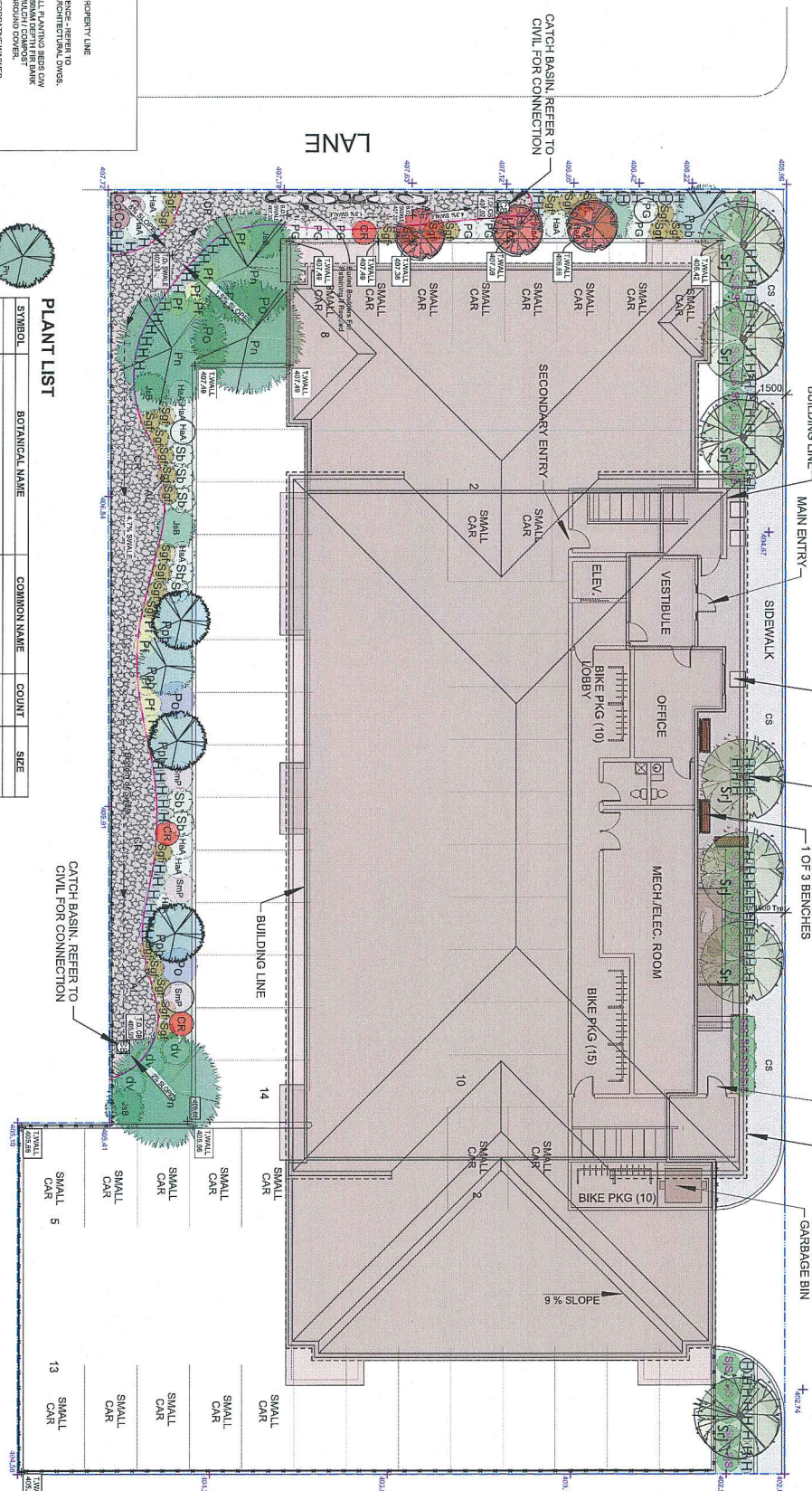
Drawn By O.S.
Checked By R.G.

Project No. 1704
Date 11/01/2017

Sheet No. AD1.1
Scale A3, NOTED



31A STREET



LEGEND

- PROPERTY LINE
- FENCE - REFER TO ARCHITECTURAL DWGS.
- ALL PLANTING BEING OW
- MINIMUM 10' SETBACK FROM DRIVEWAY
- DECORATIVE WASHED, GROUND COVER
- APPROVED NONWOVEN GEOTEXTILE OVER ON CONCRETE ASPHALT REFER TO CIVIL ENR. DWG.
- 5' PLANTER BOXES, 2' BY 2' BY 2' MINIMUM CONCRETE
- FIBERGLASS PLANTER BOX ON SUBSIDIARY PLANTING
- MINIMUM 10' PLANTING SETBACK FROM DRIVEWAY
- LANDSCAPE CATCH BASIN TO CONNECT TO DRAINAGE SYSTEM IN PARKING LOT.
- EXISTING SPOT ELEVATIONS
- PROPOSED SPOT ELEVATIONS
- PROPOSED SPOT ELEVATIONS

PLANT LIST

SYMBOL	BOTANICAL NAME	COMMON NAME	COUNT	SIZE
Pn	Prunus nigra	Australian Pine	4	2.5M HT. B&B
Pn	Picea sitchensis 'Bakeri'	Bakeri Blue Spruce	4	2.5M HT. B&B
Sj	Syringa reticulata 'Harbinica'	Japanese Tree Lilac	7	60MM CAL.
Ag	Acer glabrum	Amur Maple	3	60MM CAL.
Sh	Shrub			
dv	Cornus alba 'Ballistic'	Vigilant Dogwood	3	2 CAL. POT.
Cc	Cornus canadensis	Smoke Bush	3	2 CAL. POT.
H4A	Hydrangea arborescens 'Annabelle'	Annabelle Hydrangea	12	2 CAL. POT.
J4B	Juncus sibiricus 'Broadmoor'	Broadmoor Spirea Juniper	5	2 CAL. POT.
Pn	Philadelphus 'Salandar'	Franklin's Philadelphus	8	2 CAL. POT.
Pn	Philadelphus 'Paul Sweeney'	Paul Sweeney Philadelphus	10	2 CAL. POT.
Pn	Philadelphus 'Yellow Star Millie'	Yellow Star Millie Philadelphus	3	2 CAL. POT.
Ch	Rosa rugosa 'Champlain'	Champlain Rose	3	2 CAL. POT.
SjS	Spiraea x japonica 'Shirokawa'	Shirokawa Spirea	27	2 CAL. POT.
SjS	Spiraea x japonica 'Goldflame'	Goldflame Spirea	29	2 CAL. POT.
SjP	Syringa meyeri 'Palmeri'	Snowy Korean Lilac	7	2 CAL. POT.
Ch	Chamaenerion			
H	Helianthus scaberrimus 'Sapphire'	Blue Oak Grass	77	1 CAL. POT.

ALL WORK AND MATERIALS TO BCSLA, CNLA STANDARDS

DATE	REVISION	BY
Nov. 2, 2017	Issued for Development Permit	LF

LAZZARIN SVISDAHL
LANDSCAPE ARCHITECTS
200-200 W. 4th Avenue, Vancouver, B.C. V6B 2R2
Tel: 604-681-1111 Fax: 604-681-1112
www.lazzarin-svisdahl.com

Project: 1803-31a Street, Vernon, BC
Sheet Title: Landscape Plan
Date: Oct. 20, 2017
Scale: 1:100m
Drawn By: LSVH
Project No.:
Drawing No.: L1

Copyright reserved. This drawing and the design on and all other documents are the property of Lazzarin and Svisdahl. No part of this drawing may be reproduced or used without the written consent of Lazzarin and Svisdahl.



1803 31A Street Development

Parking Study

Prepared for: **Ironclad Developments Inc.**

Prepared by: **Watt Consulting Group**

Our File: **2290**

Date: **December 20, 2017**

TABLE OF CONTENTS

1.0	INTRODUCTION	1
1.1	Subject Site.....	1
1.2	Site Characteristics	2
2.0	PROPOSED DEVELOPMENT	5
2.1	Land Use	5
2.2	Proposed Parking Supply.....	5
3.0	PARKING REQUIREMENT	5
4.0	EXPECTED PARKING DEMAND	6
4.1	Resident Parking Demand	6
4.2	Visitor Parking Demand	9
4.3	Summary of Expected Parking Demand	10
5.0	ON-STREET PARKING CONDITIONS	10
6.0	CARSHARING	11
7.0	SUMMARY	13
7.1	Recommendations	13

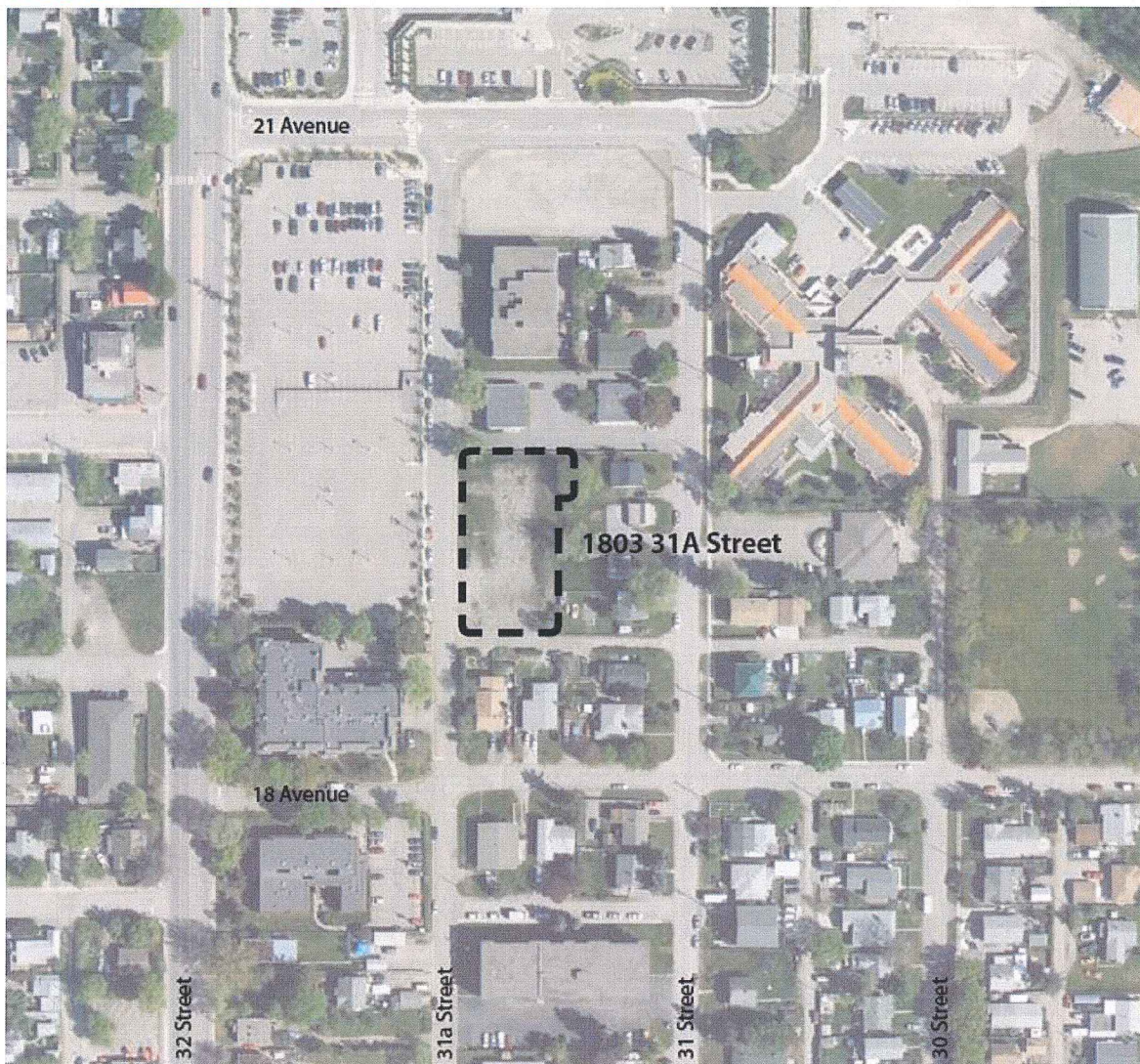
1.0 INTRODUCTION

Watt Consulting Group (“WATT”) was retained by Ironclad Developments Inc. to conduct a parking study for the proposed rental housing development at 1803 31A Street in the City of Vernon. The purpose of this study is to determine whether the proposed parking supply will accommodate demand, in consideration of on-site parking management and transportation demand management options.

1.1 SUBJECT SITE

The proposed development site is located at 1803 31A Street in the City of Vernon. The site is currently zoned RH-1 | Low-Rise Apartment Residential. See **Figure 1**.

FIGURE 1. SUBJECT SITE



1.2 SITE CHARACTERISTICS

The following provides details regarding transportation options and services that are located in close proximity to the site. See **Figure 2**.



Services

The site is located within a one (1) minute walk of Vernon Jubilee Hospital, a pharmacy and a nearby restaurant. Downtown Vernon is located within a 20 minute walk, where residents can access grocery stores, postal services and other amenities. There is also a recreational centre and library south of downtown which is also within a 20 minute walk of the site. The Armoury Park and the Polson Park Trail, which connects to Polson Park, are within a five (5) minute walk of the site and provide park space for recreational activities (i.e. baseball, soccer, and playground).



Transit

The closest bus stop to the site is located on 15th Avenue and serves Route 6 | College which provides a connection between Okanagan College, the Hospital, and the downtown transit exchange. Another bus stop located on 34th Street, near 19th Avenue, (within a five (5) to seven (7) minute walk of the site), serves Route 5 | South Vernon which provides a connection between the residential areas in the south and the downtown transit exchange. Both routes are classified as local routes, with a service frequency of 20 to 120 minutes.¹

According to the North Okanagan Transit Future Plan (2013)², Route 6 is to be rerouted along Highway 97 as part of a frequent transit corridor. With the realignment, the bus stop near the hospital will be relocated to Highway 97, but will still be within a reasonable walking distance of the site. Service frequency during peak weekday hours will be improved to 20 minutes and service on weekends will be every 30 to 60 minutes.



Walking

There is limited sidewalk infrastructure surrounding the site, with sidewalks on only one side of the road, or there are no sidewalks available. A sidewalk will be installed in front of the subject site, however there are no sidewalk connections adjacent to the subject site. A crossing, if warranted, should be considered to provide a connection to the sidewalk on the west side of 31a Street. The site has a walkscore of 53³ which suggests that the site is somewhat walkable and

¹ Regional District of North Okanagan Agenda. Available online at: http://www.rdno.ca/agendas/120104_cow_agm.pdf

² North Okanagan Transit Future Plan. Available online at: <https://bctransit.com/servlet/documents/1403641053613>

³ Walkscore. Available online at: <https://www.walkscore.com/score/1803-31-st-vernon-bc-canada>

pedestrians can complete some errands by foot. The Polson Trail is also located close to the site (within a 5 minute walk) which connects to Polson Park and several retail stores near Polson Drive and Kalamalka Lake Road. There is a multi-use trail south of the site that connects to Okanagan College.



Cycling

The closest cycling route is located 300 meters away, and is a multi-use trail that connects to Okanagan College and the west side of Vernon to residential areas. There are also bike lanes located 400 meters away, across Highway 97 on 34th Street. Both routes are a short two (2) to three (3) minute bike ride from the subject site. There is no planned cycling infrastructure that is accessible within the vicinity of the site.⁴



Carsharing

There are no existing carshare services available in Vernon; however, OGO Carshare Co-op (OGO) is considering expanding into Vernon. Ironclad Development Inc., the developer of this site, is working with OGO to consider a carshare program as part of this development. A carshare program would be beneficial to site residents that require a vehicle on an infrequent basis.



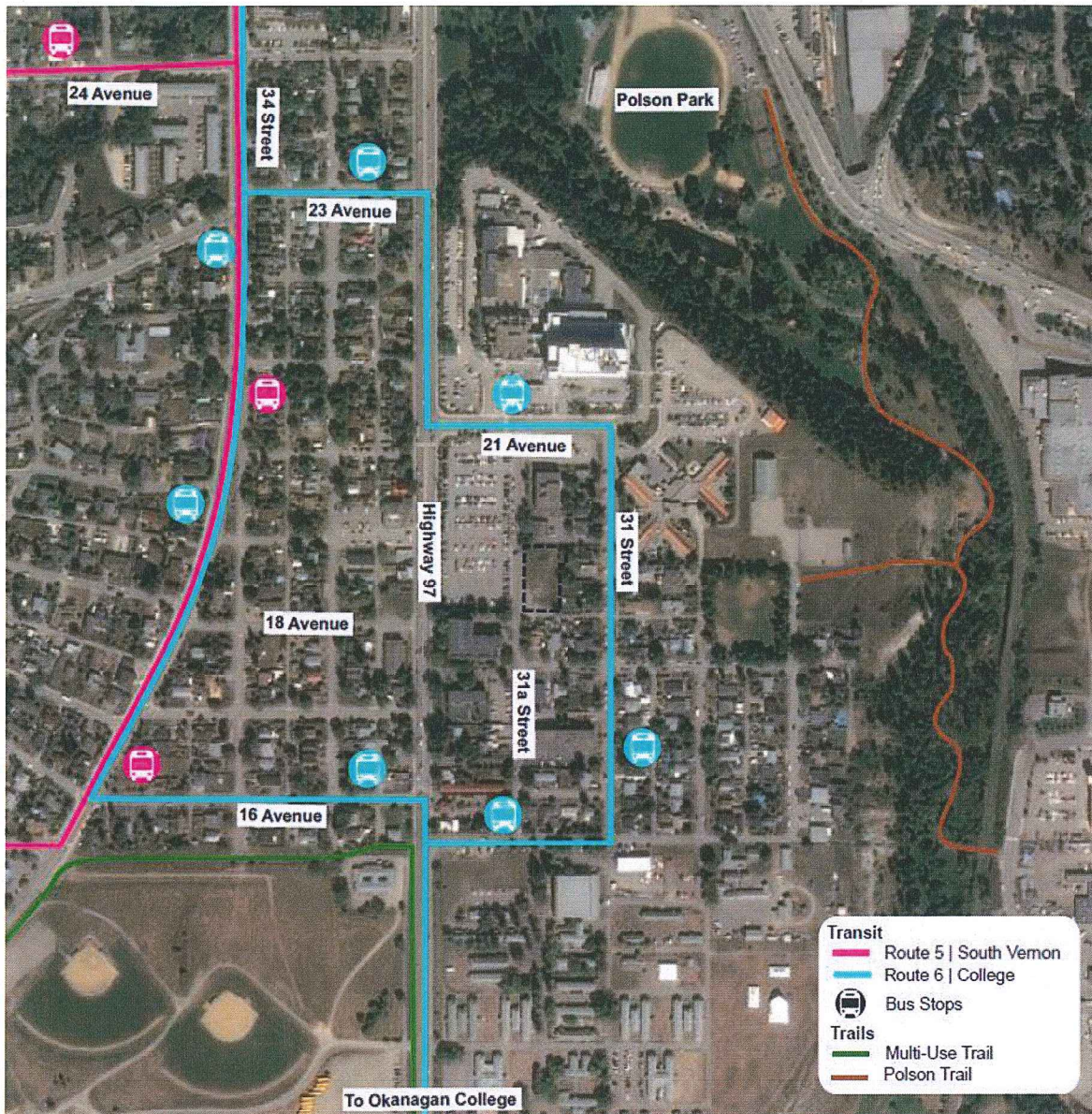
Target Market

The site is located within a one (1) minute walk to the Vernon Jubilee Hospital which provides employees an opportunity to live close to where they work. Staff at the hospital, particularly care-aids, nurses and doctors work odd hours and would benefit from such a short commute. The hospital is a large employment generator, suggesting there would likely be a high demand for units at this site. Another target market may be seniors as health care is a service that they may require more often than the general populace. The site may also be well suited for family and caregivers of patients in the hospital.

The site is also located approximately two (2) and a half kilometers away from Okanagan College. Students may be attracted to the convenience of the location as it would be a 10 minute bus ride to school via Route 6, or a 15 minute bike ride.

⁴ 25 Year Master Transportation Plan. Available online: https://www.vernon.ca/sites/default/files/docs/planning-building/150713_2015_mtp_forocpamendment.pdf

FIGURE 2. TRANSPORTATION OPTIONS MAP



2.0 PROPOSED DEVELOPMENT

2.1 LAND USE

The proposed development is for 43 market rental apartment units. See **Table 1**. Units will be one-bedroom, two-bedroom and three-bedroom units ranging in size from 532 sq.ft. to 1,078 sq.ft.

TABLE 1. SUMMARY OF PROPOSED DEVELOPMENT

Unit Type	Floor Area	Quantity
One Bedroom	532 sq.ft. to 582 sq.ft.	11
Two Bedroom	836 sq.ft. to 867 sq.ft.	20
Three Bedroom	1,042 sq.ft. to 1,078 sq.ft.	12
Total Units		43 Residential Units

2.2 PROPOSED PARKING SUPPLY

The proposed parking supply is for 54 spaces, a parking supply rate of 1.29 spaces per unit. The development also includes 35 Class I bike spaces and 11 Class II bike spaces.

3.0 PARKING REQUIREMENT

The City of Vernon's Zoning Bylaw⁵ determines the minimum parking supply requirement. The 'Apartment Housing' requirement would apply to the multi-family residential use and results in a total requirement of 74 parking spaces. See **Table 2**.

TABLE 2. SUMMARY OF PARKING REQUIREMENT

Unit Type	Quantity	Parking Requirement (Rate)	Parking Requirement (Quantity)
One Bedroom	11	1.25 spaces / unit	14
Two Bedroom	20	1.5 spaces / unit	30
Three Bedroom	12	2.0 spaces / unit	24
Visitor	43	1 space / 7 units	6
Total Parking Requirement			74 Spaces

⁵ City of Vernon Zoning Bylaw No. 5000, Section 7: Parking and Loading. Available online at: https://www.vernon.ca/sites/default/files/docs/bylaws/zoning_7_parking_loading.pdf

4.0 EXPECTED PARKING DEMAND

This section provides a summary of local data and secondary research to determine expected resident and visitor parking demand.

4.1 RESIDENT PARKING DEMAND

4.1.1 REPRESENTATIVE SITES

Sites were selected that were deemed representative of the subject site to determine an estimated parking demand for the proposed site. Study sites are located within close proximity to the subject site or exhibit similar characteristics in terms of access to services, public transit and cycling routes. The Walk Score was obtained for each representative site and ranges between 48 and 90. See **Table 3**.

TABLE 3. REPRESENTATIVE SITE CHARACTERISTICS

Location	Tenure	Number of Units	Walk Score	Distance to Site
1604 31st Street	Apartment Rental	49	48	90m
1903 31a Street	Apartment Rental	24	56	280m
2310 34 Street	Apartment Rental	40	84	750m
2312 36th Street	Apartment Rental	22	64	1.4km
2800 35 Street	Apartment Rental	42	90	1.4km
2313 36th Street	Apartment Rental	12	70	1.4km
3806 24 Avenue	Apartment Rental	26	58	1.7km

OBSERVATIONS

Observations were conducted on Wednesday November 22nd and Thursday November 23rd between 7:30pm and 8:30pm⁶. All representative sites have surface parking, which allowed for fast and efficient counts of parked vehicles.

Results suggest an 85th percentile peak parking demand of 0.72 vehicles per unit, with rates ranging from 0.41 vehicles per unit to 0.95 vehicles per unit. See **Table 4**.

⁶ As the observations were conducted from 7:30pm to 8:30pm (not considered the ultimate peak for residents) a 5% factor was added to results of observations to reflect peak residential parking demand when resident parking demand is at 100%, as per the *ULI Shared Parking Manual*

TABLE 4. SUMMARY OF OBSERVATIONS AT REPRESENTATIVE SITES

Location	Number of Units	Wednesday November 22, 7:30pm		Thursday November 23, 7:30pm	
		Vehicles Observed	Demand Rate (vehicles per unit)	Vehicles Observed	Demand Rate (vehicles per unit)
1903 31a Street	24	11	0.46	14	0.58
1604 31 st Street	49	28	0.57	26	0.53
2310 34 th Street	40	39	0.98	38	0.95
3806 24 th Avenue	26	11	0.42	11	0.42
2800 35 th Street	42	28	0.67	29	0.69
2312 36 th Street	22	13	0.59	9	0.41
2313 36 th Street	12	6	0.50	8	0.67
		Average	0.60		0.61
		85th Percentile	0.70		0.72

VERBAL SURVEYS

Property management companies were contacted⁷ for the each of the representative sites to inventory the number of resident vehicles at each site. Response rate was low for this exercise, and only property managers from three of the representative sites were successfully contacted. The number of vehicles for each of the three sites was an average of 40% higher than the number of vehicles observed during the busier of the two observations at those same sites (see above). This factor was then applied to the other apartment sites where a site manager was not successfully contacted. See **Table 5**.

Average parking demand is 0.89 vehicles per unit and the 85th percentile demand is 1.15 vehicles per unit.

⁷ Phone calls were made on Monday November 17 and Tuesday November 18, 2016

TABLE 5. SUMMARY OF VERBAL SURVEY AT REPRESENTATIVE SITES

Site	No. Units	No. Vehicles	Demand Rate (vehicles/unit)
1903 31a Street	24	-	0.81*
1604 31 st Street	49	-	0.80*
2310 34 th Street	40	45	1.13
3806 24 th Avenue	26	-	0.59*
2312 36 th Street	22	-	0.83*
2800 35 th Street	42	30	0.71
2313 36 th Street	12	16	1.33
		Average	0.89
		85th Percentile	1.15

*These parking demand rates were adjusted to reflect a higher parking demand based on surveys compared to observations

4.1.2 PARKING DEMAND BY UNIT TYPE

Research suggests that parking demand varies based on the size of unit - the higher the number of bedrooms, the higher the parking demand. Study sites have been factored to account for unit configuration (i.e., number of bedrooms) as follows:

1. Parking demand at each site
2. The breakdown of unit type (i.e., number of bedrooms) at each site; and
3. The assumed “ratio differences” between each unit type based on the King County Metro⁸ study which recommends one-bedroom units have a 20% higher parking demand than bachelor units, two-bedroom units have a 60% higher parking demand than one-bedroom units, and three-bedroom units have a 15% higher parking demand than two-bedroom units.

Average parking demand and 85th percentile parking demand were considered among each representative site. See **Table 6**. When factored for unit configuration, using 85th percentile as a conservative measure, the results conclude the following:

- One-Bedroom Units (11) = 0.81 vehicles per unit, 9 vehicles
- Two-Bedroom Units (20) = 1.14 vehicles per unit, 23 vehicles
- Three-Bedroom Units (12) = 1.40 vehicles per unit, 17 vehicles
- Total Vehicles = 49 vehicles

⁸ King County Metro. (2013). Right Size Parking Model Code. Table 2, page 21.
Available online at: <http://metro.kingcounty.gov/programs-projects/right-size-parking/pdf/140110-rsp-model-code.pdf>

TABLE 6. SUMMARY OF PARKING DEMAND BY UNIT TYPE

Site	No. Units	Peak Vehicle Demand	Bachelor Rate	One Bedroom Rate	Two Bedroom Rate	Three Bedroom Rate
1903 31a Street	24	0.81		0.74	1.18	
1604 31st Street	49	0.80	0.52	0.65	1.04	
2310 34 Street	40	1.13				1.13
3806 24 Avenue	26	0.59	0.50	0.63		
2312 36th Street	22	0.83		0.83		
2800 35 Street	42	0.71	0.46	0.58	0.92	
2313 36th Street	12	1.33	0.64	0.81		1.45
Average		0.89	0.53	0.71	1.05	1.29
85th Percentile		1.15	0.59	0.81	1.14	1.40

4.2 VISITOR PARKING DEMAND

Observations of designated visitor parking spaces were conducted concurrently with the site observations on November 22nd and November 23rd. See **Table 7**. Visitor parking was identified and observed at two sites. Results suggest a peak average visitor parking demand of 0.1 visitor vehicles per unit. The findings are similar to the *Metro Vancouver Apartment Parking Study*⁹ that concluded that typical visitor parking demand is less than 0.1 vehicles per unit.

TABLE 7. SUMMARY OF VISITOR PARKING DEMAND

Location	Number of Units	Wednesday November 22, 7:30pm		Thursday November 23, 7:30pm	
		Vehicles Observed	Demand Rate (vehicles per unit)	Vehicles Observed	Demand Rate (vehicles per unit)
1903 31a Street	24	1	0.04	4	0.17
2800 35 th Street	42	1	0.04	1	0.04
Average			0.04		0.10

⁹ Metro Vancouver Apartment Parking Study, Technical Report, 2012. Available online at: http://www.metrovancouver.org/services/regional-planning/PlanningPublications/Apartment_Parking_Study_TechnicalReport.pdf

4.3 SUMMARY OF EXPECTED PARKING DEMAND

Expected parking demand is approximately 53 vehicles, which is one less than the proposed parking supply. See **Table 8**.

TABLE 8. SUMMARY OF EXPECTED PARKING DEMAND

	Units	Expected Parking Demand	
		Rate	Total
Resident One Bedroom	11	0.81 vehicles per unit	9
Resident Two Bedroom	20	1.14 vehicles per unit	23
Resident Three Bedroom	12	1.40 vehicles per unit	17
Visitor	43	0.1 vehicles per unit	4
Total Expected Parking Demand			53

5.0 ON-STREET PARKING CONDITIONS

On-street parking conditions were observed within close proximity to the site, bounded by 21st Avenue to the north, 30th Street to the east, 15th Avenue to the south and 32nd Street to the west. Parking restrictions on these road segments are either unrestricted or there is no parking available. See **Appendix A** for a summary of observations.

Observations¹⁰ were completed on two weekday evenings and one weekday afternoon to reflect the anticipated “peak” periods for the site (residential) and for the surrounding neighbourhood (hospital). Observations were conducted during the following time periods:

- Wednesday November 22, 7:30pm
- Thursday November 23, 7:30pm
- Tuesday November 28, 1:30pm

Total occupancy on the peak weekday evening observation (Thursday November 23rd) is 46% with 82 spaces remaining for residents and visitors. Other notable on-street parking trends during the peak observation period (for the site) include the following:

- 31a Street between 21st and 18th Avenue (directly adjacent the site) was observed at 100% occupancy;
- 31st Street between 21st and 18th Avenue was 80% occupied with three spaces still available; and
- 18th Avenue from 31a Street to 31st Street was observed at 31% occupancy with nine spaces still available.

¹⁰ A 5% factor was added to the results of the weekday evening observations to reflect peak residential parking demand based on the ULI Shared Parking Manual.

Total on-street occupancy was highest during the mid-day observation with a total occupancy of 63% with 57 spaces unoccupied. This increased occupancy during the day is directly attributed to spillover parking from the Hospital. As weekday afternoon is not the peak demand period for residents, this is not expected to impact the ability of residents or visitors of the site to park on-street.

Although spillover is not expected from the site, there is capacity to accommodate visitors or residents of the site within two blocks without displacing existing vehicles.

6.0 CARSHARING

The applicant is considering the implementation of a carshare program at the site utilizing Okanagan Carshare Co-op, "Ogo" the carshare corporation operating in the Okanagan. There is currently no carshare vehicle located in close proximity to the site that would target site residents. It is recommended that the applicant purchase a vehicle that should be located on-site that will be available to site residents as well as all Ogo members. The cost of the vehicle will be approximately \$34,000. Although providing a vehicle may encourage usage among employees; providing the combination of a vehicle and memberships will see the largest uptake by residents. Ogo has two membership options including the following:

1. Member-Owner. Appropriate for members who will use the program on a regular basis. Up-front costs include a \$500 refundable member share and a \$25 sign-up fee. The hourly rate is \$5/hour and mileage fees of \$0.40/km and \$0.25/km after 50km.
2. Casual. Appropriate for members who want to "try out" the carsharing program. This option does not require a refundable member share. It includes the same sign-up costs, hourly costs and mileage costs as the member-owner option. There is also a \$25 monthly fee.

The member-owner is the most applicable membership option to site residents, however, the refundable member share may be a barrier to some. The applicant is proposing to purchase memberships for the two- and three-bedroom units (32 units in total) which will have a cost of \$16,000. Residents will be responsible for the sign-up fee, hourly fees and mileage fees. Memberships should be tied to units, to ease the management during turnover phases of residents. The site is an ideal location for residents to reap the benefits of a carshare program, particularly for those residents who work at the hospital. The carshare program would "fill the gap" and provide residents an opportunity to have access to a vehicle on an as-needed basis.

A comprehensive study in California found a decrease in average vehicle ownership from 0.47 to 0.24 vehicles per household¹¹ (accounts for a range of housing types) among households that joined carshare services, an approximately 50% reduction in vehicle ownership.

¹¹ Martin and Shaheen, The Impact of Carsharing on Household Vehicle Ownership, Access Magazine, Spring 2011, pg22

A study of carshare programs in the City of Toronto found that vehicle ownership rates at condominium sites without carshare vehicles was 1.07 vehicles per unit, whereas buildings with one or more carshare vehicles had significantly lower rates at 0.53 vehicles per unit, which represents a 50% reduction in vehicle ownership rates.¹²

Two studies from Metro Vancouver explored the impact of carsharing on vehicle ownership. Over 3,400 carshare households participated in the study. The key findings are as follows:

- On average, up to 3 private personal vehicles were shed per carshare vehicle.
- A regression analysis found that those living in rental housing and in a smaller household size are statistically more likely to give up vehicle ownership compared to the reference case.¹³
- The number of carshare vehicles within walking distance has a small but statistically significant relationship with apartment household vehicle holdings.¹⁴

Other jurisdictions recognize the decrease in parking demand associated with carsharing in their development regulations. The City of Vancouver, as an example, allows for a reduction of five spaces for each carshare vehicle purchased and parked on-site¹⁵, where a model regulation for King County (Seattle) suggests a reduction of four spaces¹⁶. Similar regulations are in-place in New Westminster, Coquitlam, and Richmond allowing for a 5-15% reduction where carshare vehicles are accessible. A study by the City of Toronto recommends a reduction of one parking space for buildings with less than 30 units¹⁷. Correspondence from Victoria Carshare Cooperative (now Modo)¹⁸ suggests a 5-10% reduction in parking demand where memberships are provided and a vehicle easily accessible, and a similar reduction of 5-10% is recommended in *Parking Management Best Practices*¹⁹.

A proposed development in Penticton (135 Front Street) is coordinating with OGO to implement a car sharing program at the site. The proposal is for 14 residential units vehicle²⁰ and ground-floor commercial with 2 parking spaces on-site (there are also six additional parking spaces on a lot adjacent the site). In order to support this parking variance, the applicant is providing one carshare vehicle and one reserved carshare vehicle space on-site. The Penticton Zoning Bylaw

¹² City of Toronto. (2009). Parking Standards Review: Examination of Potential Options and Impacts of Car Share Programs on Parking Standards. Available online at:
https://www1.toronto.ca/city_of_toronto/city_planning/zoning_environment/files/pdf/car_share_2009-04-02.pdf

¹³ Ibid, pg. 54.

¹⁴ Metro Vancouver. (2014). The Metro Vancouver Car Share Study: Technical Report. Available online at:
<http://www.metrovancouver.org/services/regional-planning/PlanningPublications/MetroVancouverCarShareStudyTechnicalReport.pdf>

¹⁵ Refer to City of Vancouver Bylaw no.6059, Section 3.2.2, available at: <http://vancouver.ca/your-government/parking-bylaw.aspx>

¹⁶ King County Metro, Right Size Parking Model Code, December 2013, pg21, available at:
<http://metro.kingcounty.gov/programs-projects/right-size-parking/pdf/140110-rsp-model-code.pdf>

¹⁷ City of Toronto, Parking Standards Review, IBI Group, 2009

¹⁸ Correspondence from Victoria Carshare Cooperative (now Modo), received August 2009

¹⁹ T Litman, Parking Management Best Practices, American Planning Association, 2007

²⁰ Discussion with OGO Carshare Co-op Representative on December 11, 2017.

recognizes this program and supports a reduction of three spaces for each carshare vehicle and space²¹.

Based on the research and the representative site in Penticton, a reduction of four parking spaces will be supported if a carshare vehicle is placed on site and memberships are provided for the two- and three-bedroom units. This reduces expected parking demand to 49 vehicles plus one reserved space for a carshare vehicle.

7.0 SUMMARY

The proposed development is for 43 units and 54 off-street parking spaces – a parking supply rate of 1.29 spaces per unit. The City's Parking Bylaw identifies a required minimum parking supply of 74 parking spaces, which is 20 more than proposed.

Expected parking demand was calculated for the site based on observations and verbal surveys. Results suggest an expected parking demand of 49 resident vehicles and four visitor vehicles, which is a total site demand of 53 vehicles. Site parking demand is expected to be accommodated within the proposed off-street parking supply without impacting the surrounding neighbourhood. With the implementation of a carshare program, expected parking demand is reduced to 49 vehicles (plus one reserved parking space for a carshare vehicle).

7.1 RECOMMENDATIONS

1. It is recommended the City grant the requested variance to allow for provision of 54 parking spaces (1.29 spaces per unit).

²¹ 123 Front Street, Penticton Council Minutes March 23, 2017. Available online at: [http://www.penticton.ca/assets/City-News/News/2017/2017-03-23%20Public%20Notice%20\(135%20Front\).pdf](http://www.penticton.ca/assets/City-News/News/2017/2017-03-23%20Public%20Notice%20(135%20Front).pdf)

APPENDIX A. ON-STREET PARKING ASSESSMENT



201-460 DOYLE AVENUE
KELOWNA, BC V1Y 0C2
(250) 469-6617
INFO@OGOCARSHARE.CA
WWW.OGOCARSHARE.CA

Agreement

Kelowna, British Columbia – January 17th, 2018

This agreement outlines the commitment between the Okanagan Car Share Co-op (OGO) and IRONCLAD DEVELOPMENT INC to integrate carsharing into the development at 1803 31A Street, in Vernon, BC.

BETWEEN:

**Okanagan Car Share Co-op
doing business as OGO**
201 – 460 Doyle Ave,
Kelowna, B.C.
V1Y 2A2
("OGO")

AND

**IRONCLAD DEVELOPMENTS INC
doing business as Developer**
101 – 57158 Symington Rd 20E
Springfield, MB
R2J 4L6

("Developer")

WHEREAS:

- A. Developer has undertaken the residential development of the lands located at 1803 31A Street in the City of Vernon, British Columbia;
- B. OGO is a not-for-profit co-operative that fosters carsharing and raises awareness about the benefits of sharing cars over individual ownership;
- C. Developer will designate one (1) designated above ground parking space at 1803 31A Street (the "Co-op Space") for the exclusive use of the Vehicle (as defined hereafter), substantially as shown in Appendix 1;
- D. OGO will, at its cost, operate, maintain, repair and insure the Vehicle(s) and administer the carsharing program (the "Carsharing Program"); and
- E. Developer and OGO intend that the Vehicle(s) will be available for use by all OGO members, including the residents of 1803 31A Street who become members of OGO, collectively, the "Permitted Users").



201-460 DOYLE AVENUE
KELOWNA, BC V1Y 0C2
(250) 469-6617
INFO@OGOCARSHARE.CA
WWW.OGOCARSHARE.CA

F. OGO, in its sole discretion, may assign, pledge or transfer this Agreement or the proceeds therefrom with the developer's consent. Developer may not assign this agreement without OGO's consent. This Agreement shall be binding upon and endure to the benefit of OGO and developer's respective successors and assigns.

NOW THEREFORE in consideration of the sum of Ten (\$10.00) Dollars now paid by each party to the other (the receipt and sufficiency whereof is by each hereby acknowledged) and in consideration of the mutual promises and covenants contained herein, the parties agree as follows:

Project Fee

1. Prior to completion of construction of Development, Developer will pay to OGO the sum of \$ 51,000 (the "Project Fee"), which sum is inclusive of taxes, shipping and all other fees and charges, for the purchase and carsharing set up of one (1) Vehicle equipped for carsharing purposes upon the execution of this Agreement by the parties. Parts of the fund will also be applied towards the cost of maintaining, operating, insuring and administering the Vehicle(s) during the Term (as defined hereafter).
2. Another carshare vehicle will act as a neighbourhood amenity that will be placed within a five hundred (500) meter radius from the development at a highly visible parking spot that is yet to be determined. The developer agrees to take on negotiations with the City of Vernon for finding a suitable location together with OGO.
3. The project fee also includes thirty two (32) non-refundable co-op memberships (each worth \$500) that will be assigned to each unit. Those members still need to meet the co-op's eligibility criteria to become members. The memberships will remain with the unit for an undefined time period.
4. OGO will use the Project Fee forthwith upon receipt to purchase the carshare vehicles (the "Vehicle(s)") for use as the Vehicle(s), and will provide Developer with a copy of the vehicle registration evidencing that the Vehicle is registered in the name of OGO together with proof of insurance.
5. Upon receipt of the Project Fee, OGO will issue a receipt to Developer confirming payment of the Project Fee to OGO.
6. Developer agrees that OGO will be the sole provider of the Carsharing Program in respect of the Vehicle(s) during the Term.
7. Developer and OGO agree that the Vehicle(s) will be accessible to and useable by all members of OGO, including any residents of strata lots in the Development who become OGO members, and OGO will take all commercially reasonable steps to ensure the Vehicle is used only by the Permitted Users.



201-460 DOYLE AVENUE
KELOWNA, BC V1Y 0C2
(250) 469-6617
INFO@OGOCARSHARE.CA
WWW.OGOCARSHARE.CA

8. OGO agrees to provide usage of the Vehicle(s) for OGO members and to cause the Vehicle(s) to be parked in the nearby Co-op Space(s) at all times when not in use by an OGO member. For greater certainty, OGO will not be responsible for any costs in respect of the Co-op Space(s) during the Term, including, without limitation, 24 hour per day, 7 day per week OGO member access to the Co-op Space(s) and the maintenance of the Co-op Space(s).
9. OGO will be solely responsible for providing and paying for the Carsharing Program, including but not limited to the operating, administration, maintenance, repair and insurance costs.

OGO agrees to maintain, repair, and operate the Vehicle(s) and, if a Vehicle is damaged beyond repair during the Term, to replace such a Vehicle with a vehicle which is at least equivalent in value to the Vehicle such that the Vehicle is always available for use by the Permitted Users during the Term.

10. OGO acknowledges and agrees that Developer will not be responsible for any costs associated with the Vehicle(s) or the Carsharing Program, including without limitation, any applicable taxes or delivery fees in respect of the purchase of the Vehicle(s), beyond the payment of the Project Fee.
11. Developer acknowledges and agrees that residents of Development (the "Residents") will not automatically become members of OGO and must meet OGO's registration requirements and join OGO in order to be eligible to use the Vehicle(s) and the Carsharing Program.
12. Developer acknowledges and agrees that Residents who are OGO members will be responsible for applicable member fees in respect of the use of the Vehicle(s), such member fees as noted on the OGO website.
13. OGO will provide the Carsharing Program for a term of two (2) years (the "Term") commencing from the date of issuance of the first occupancy permit for any new buildings, improvements or structures, which include dwelling units, constructed at 1803 31A Street (and any renewal term, if applicable), following which this Agreement may be terminated by either part upon obtaining the prior written consent of the other party to this Agreement, provided that this Agreement may not be terminated by either party unless such a party obtains the prior written consent of the City of Vernon to vary the requirements set out in the Development Permit to provide for the Vehicle, the Co-op space and the Carsharing program at 1803 31A Street.
14. An annual review will take place at the date of the first and second anniversaries of signing of the Agreement to assess the profitability of providing carsharing at 1803



201-460 DOYLE AVENUE
KELOWNA, BC V1Y 0C2
(250) 469-6617
INFO@OGOCARSHARE.CA
WWW.OGOCARSHARE.CA

31A Street. If OGO determines that the location has achieved profitability, based on the full costs to provide carsharing at 1803 31A Street, including vehicle operating expenses, OGO would keep the vehicle close to the development after the term of this agreement.

Marketing and Promotion

15. OGO and the developer shall allow use of each other's graphics in advertising and promotional activities. Use of graphics must be in a manner whereby the graphics remain in their original form and approved by each party in writing.
16. OGO and the developer shall only use each other's trademarks, logos or trade names in connection with 1803 31A Street activities. Any other use must receive the prior written approval of each party (by email).
17. OGO will provide residents of 1803 31A Street with \$120 in driving credit to be applied to usage and mileage charges to expire three months after registration.

IN WITNESS WHEREOF

The parties have agreed to this letter of intend on the 17 day of January, 2018.

Okanagan Car Share Co-op
doing business as OGO

Ironclad Developments Inc.
doing business as developer

By: _____
Authorized Signatory

By: _____
Authorized Signatory



201-460 DOYLE AVENUE
 KELOWNA, BC V1Y 0C2
 (250) 469-6617
 INFO@OGOCARSHARE.CA
 WWW.OGOCARSHARE.CA

Appendix 1: Parking spot for carsharing vehicle

